

TOWN OF COCHRANE

# WESTERN HERITAGE DESIGN FRAMEWORK

FALL 2022





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## PREPARED FOR:

Town of Cochrane  
101 RancheHouse Road  
Cochrane, AB  
T4C 2K8

## PREPARED BY:

ERA Architects Inc.  
625 Church Street, Suite 600  
Toronto, ON  
M4Y 2G1

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**PREPARED BY** PE / SL / EA / SC / ZC

*Cover: ERA 2020*



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*Fig. 2: Tim Hall*



# 1.0 INTRODUCTION

## 1.1 Intro to the Western Heritage Design Framework (2022)

The Western Heritage Design Framework (2022) is an update to the Town of Cochrane’s Western Heritage Design Guidelines (November 2000).

In 2019, the Town of Cochrane initiated a comprehensive review of the Western Heritage Design Guidelines to ensure that they would remain current and effective.

The intent of the updated Western Heritage Design Framework is to guide the expression of Western Heritage in the public realm and on private properties in order to foster strategic placemaking throughout all of Cochrane.

Its goal is to balance the expression of Western Heritage with the evolution of Cochrane as a contemporary place in the 21<sup>st</sup> century.

The Western Heritage Design Framework recognizes that Cochrane is a place that is growing and evolving to meet the contemporary needs of locals and visitors; and that the expression of Western Heritage defines Cochrane as a unique place to live, work and play.

### **Why the Western Heritage Design Framework?**

The Western Heritage Design Framework is more than a set of detailed guidelines to establish and maintain an architectural style. This document is a framework that:

- a. recognizes the two core elements of Western Heritage;
- b. establishes Character Areas where those elements of Western Heritage will be expressed in distinct ways;
- c. sets out Design Objectives that must be met to achieve the expression of Western Heritage in each Character Area;
- d. offers supplementary Design Guidelines to help users understand how to achieve each Objective.

#### **What is Western Heritage?**

Western Heritage is an *approach to the design of the built environment* that is intended to communicate Cochrane’s identity as a unique place.

Prior to 2022, Western Heritage was understood as a single architectural style. Based on a series of public engagement exercises in 2019, the concept of Western Heritage has been revised to reflect two core elements: *Cochrane’s History*, and *Small-Town Character*.

The Western Heritage design approach is intended to convey these two core elements throughout the built environment, through a variety of design strategies for buildings, streetscapes, and outdoor spaces.



## 1.2 How to Use this Framework

The Western Heritage Design Framework exists to ensure that Cochrane’s identity as a unique place is conserved and communicated as Cochrane grows and evolves.

To achieve this goal, this document provides direction for all non-residential and mixed-use (residential-commercial) development projects in three Character Areas:

- The Old Town;
- The Railway Transition Zone; and
- Cochrane’s Corridors.

Any development project proposed in these Character Areas will be evaluated by the Town of Cochrane using the Western Heritage Design Framework.

Public and private development proposals and plans submitted to the Town of Cochrane will be expected to meet the Design Objectives in this document.

### Development Projects

- new construction
- additions on existing buildings
- alterations to existing buildings
- public works
- creation or renovation of public spaces

### How does the Western Heritage Design Framework apply to my property?

Section 3.0 maps the three **Character Areas** for the expression of Western Heritage in Cochrane. Use the maps in this section to identify whether your property is located in one of the three Character Areas.

Section 4.0 identifies the **Design Objectives** that apply to each Character Area. All development projects are expected to meet the Design Objectives for the Character Area in which they are located.

Supplementary **Design Guidelines** are provided with each Objective to assist in understanding how the Objectives can be achieved. Compliance with the Design Guidelines is not mandatory as long as the proposal can demonstrate that all of the Character Area’s Design Objectives that are applicable to the proposal are achieved.

For assistance interpreting any of the terms used throughout this document, see the **Definitions** in Section 6.0.

### **1.3 Municipal Policy Framework**

The Western Heritage Design Framework is one of several planning documents that directs how Cochrane will look, grow and evolve in the coming decades.

It is an appendix to Cochrane’s Land Use Bylaw, with a scoped objective to support placemaking via heritage conservation and interpretation in strategic locations throughout the town of Cochrane.

The Western Heritage Design Framework is nested within a greater policy framework, including:

#### **Cochrane Municipal Development Plan (2008)**

The Municipal Development Plan (2008) sets out a direction for land use and future growth in the town of Cochrane, and provides policies to achieve this direction.

Section 12 of the Municipal Development Plan provides the policy framework for the conservation of historic resources throughout Cochrane.

It directs the Town to encourage historic resource conservation through a series of programs and initiatives, including the Cochrane Heritage Register, and provides for municipal historic resource designation and heritage development review processes in accordance with the *Alberta Historical Resources Act*.

#### **Cochrane Land Use Bylaw (2022)**

The Land Use Bylaw (2022) implements the vision in the Municipal Development Plan (2008) by providing land use designations and built form direction for all properties in Cochrane.

The Land Use Bylaw provides specific zoning direction on permitted heights, maximum gross floor areas, building setbacks, parking requirements, and more.

The Western Heritage Design Framework is appended to the Land Use Bylaw (2022). Where there are conflicts between the Land Use Bylaw and the Western Heritage Design Framework, the stricter regulation shall prevail.

#### **Cochrane Integrated Neighbourhood Design Guidelines (2013)**

The Integrated Neighbourhood Design Guidelines (2013) provide a set of design guidelines for new and existing residential neighbourhoods in Cochrane.

They guide the development of Area Structure Plans, Neighbourhood Plans, and site-specific Development Permits to achieve great neighbourhoods throughout Cochrane.

There may be sites where the Integrated Neighbourhood Design Guidelines apply to properties within the Western Heritage Design Framework Character Areas. These documents do not provide conflicting direction, and in such cases, both policy frameworks would apply.



## 1.4 Complementary Policies & Initiatives

There are two key initiatives that the Town of Cochrane may explore to accompany the 2022 Western Heritage Design Framework.

### Heritage Interpretation Fund

The Town of Cochrane may explore developing a policy establishing and governing the administration of a Heritage Interpretation Fund (HIF) program.

The Heritage Interpretation Fund policy would address:

- A method of calculating cash contributions in lieu of on-site heritage interpretation in the Railway Transition Zone and Cochrane's Corridors;
- A schedule for the commitment to and collection of a Heritage Interpretation contribution within the development application process;
- A method of administration of the HIF;
- A decision process for the allocation of HIF funding to public interpretive installations;
- A recommended formalized relationship between the HIF and a town-wide Heritage Interpretation Plan (to be developed).

A Heritage Interpretation Fund is not proposed at this time, but should be considered as a future initiative to contribute in implementing town-wide heritage interpretation.

### Heritage Thematic Framework & Interpretation Plan

The Town of Cochrane is recommended to develop a town-wide Thematic Framework and Interpretation Plan.

A Heritage Thematic Framework is an historical analysis that synthesizes the valued themes of Cochrane's history. The Framework will review and reflect histories associated with Cochrane's existing historical resources, lost historic features, and intangible historic themes, practices and traditions.

A Thematic Framework can be used to inform:

- The identification of **properties for Cochrane's Heritage Register**, and properties that are recommended for municipal historic resource designation;
- The development of **on-site interpretive installations by private properties owners** in the Railway Transition Zone and Cochrane's Corridors;
- The development of a **Heritage Interpretation Plan for Cochrane**, which would establish key locations for interpretive installations to be developed using an HIF. These could include, for example, major public realm installations to mark First Street as an historic corridor, or large gateway artworks along Cochrane's Corridors.



Thematic Frameworks are developed through a combination of historical research and analysis, and meaningful public engagement, so that stakeholders can share the histories they value.

The Town of Cochrane is encouraged to partner with diverse public stakeholders (including but not limited to CHAPS, Stockman's Memorial Foundation, local Community Associations, local First Nations, and Cochrane Tourism) to develop the Thematic Framework and Interpretation Plan.



*Fig. 3: Northwest corner of First Street West and Second Avenue West, circa 1911 (Glenbow Archives PA-1365-1)*



Fig. 4: Cochrane Station, 1900s (Glenbow Archives, NA-3885-35)

## 2.0 WHAT IS WESTERN HERITAGE?

### 2.1 Understanding Western Heritage

The Western Heritage Design Framework (2022) is intended to guide the expression of Western Heritage to foster strategic placemaking throughout all of Cochrane.

**Western Heritage is an approach to the design of the built environment that is intended to communicate Cochrane’s identity as a unique place.**

Prior to 2022, Western Heritage was understood as a single architectural style, inspired by the boomtown-style architecture of Cochrane’s early settlement era.

A series of public engagement exercises in 2019 explored the meaning of Western Heritage for locals in the town of Cochrane. In identifying what they valued about Cochrane, stakeholders described two core elements of Western Heritage: **Cochrane’s History**, and **Small-Town Character**.

The concept of Western Heritage has now been revised so that these two core elements will be reflected throughout the built environment, using not just building design, but also a variety of design strategies and installations for streetscapes and outdoor spaces.

Western Heritage will be communicated primarily through heritage interpretation. Heritage interpretation refers to the range of strategies (building/public realm design, installations, media, activities) that may be used to enhance the public’s awareness and understanding of an historic place. Interpretation facilitates the communication of histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

The two core elements of Western Heritage (Cochrane’s History, and Small-Town Character) are explored further in Section 2.2.

#### **Who is the audience for Western Heritage?**

The expression of Western Heritage is aimed at both locals and visitors.

- For locals, Western Heritage makes Cochrane a liveable place and helps to reinforce its civic identity, allowing for the recognition of local histories throughout the public realm.
- For visitors, Western Heritage marks Cochrane as a unique place, distinct from other municipalities, with its own valued story that Cochranites are proud to share.

## 2.2 Core Elements of Western Heritage

There are two core elements of Western Heritage: Cochrane's History, and Small-Town Character.

### Cochrane's History

The Western Heritage design approach involves, in part, the *expression of the valued themes of Cochrane's history and cultural identity (its cultural heritage values)* throughout town.

Cochrane is situated along the Bow River on the ancestral lands of diverse indigenous peoples, including the Piikani, Blood, Siksika, Stoney/Nakoda and Tsuu T'ina Nations.

Ceded to the Dominion of Canada under Treaty 7 in 1877, the area's first colonial settlement came in 1881, when Senator Matthew Cochrane established the Cochrane Ranche Co. Ltd. on over 100,000 acres adjacent to the future Canadian Pacific Railway (CPR).

In 1885, the Cochrane town site was granted to the CPR, and subdivided into urban town lots on a grid structure on the railway's north edge. South of the railway, early industries were established to serve the surrounding ranch country, including four brickyards, coal mines, a quarry and a sawmill.

Cochrane's growth slowed during the interwar period, but in the 1950s began to expand beyond the original town site, with new residential neighbourhoods in the east, and industrial services south of the railway.

With sustained growth through the late 20<sup>th</sup>-early 21<sup>st</sup> century, Cochrane remains a contemporary commercial service centre for the surrounding ranching communities.

Key themes of Cochrane's history will be identified and synthesized through research and public engagement processes (see Section 1.4). Based on the existing historical narrative, they may include First Nations histories, early town settlement, and regional commerce and industry fostered by transportation routes like the Canadian Pacific Railway and major Albertan highways.

These themes could be understood as the community's current cultural heritage values. Cultural heritage values are not static; they evolve over time, as community values change. For example, they may shift over time to recognize the contributions of certain cultural communities, or of recent eras of architecture and design.

The Town of Cochrane's future Heritage Thematic Framework will engage stakeholders to define the key themes of Cochrane's history as they are understood and valued today.



Fig. 5: Cochrane Rail Station, c. 1910.  
(Glenbow Archives NA-4811-2)

## Small-Town Character

The Western Heritage design approach also involves, in part, the *expression of small-town character* in strategic locations throughout town. It recognizes the historic town site as a priority area for the celebration of small-town character.

The historic urban town site of Cochrane was surveyed and planned in 1885, and settled beginning in the late 1880s.

Located between the Canadian Pacific Railway (south), the Big Hill (north), the Cochrane Ranche (west), and Baird Avenue (east), Cochrane's original town site is notable for its representation of pre-World War Two urban development. Attributes of this character include:

- A rectilinear street grid;
- A small-scale, walkable block structure;
- Small-scale urban town lots;
- A mix of typical residential and commercial built forms in close proximity, ranging between 1-2 storeys in height.

Cochrane's growth in the post-World War Two period is equally representative of its era. The suburban development of the era was more automobile oriented, with commercial sites designed with parking lots as key features. Residential neighbourhoods featured curvilinear streets and cul-de-sacs to limit vehicle traffic. The dichotomy of this distinctly suburban character makes it easy to recognize Cochrane's historic urban town site.

Today, as a service centre, Cochrane's contemporary development is still designed to be accessed by vehicle. However, Cochranites value the walkability inherent in the original town site's grid structure and small scale.

Walkable small-town character and a comfortable pedestrian experience has been identified by stakeholders as a central value to inform Western Heritage. Although it is encouraged to be expressed in all new development in Cochrane, it is to be conserved and supported strategically throughout the historic town and in transition zones to contemporary commercial areas.



Fig. 6: Small town of Cochrane north of the railway, c. 1960. (Glenbow Archives PA-2807-4139)

COCHRANE  
Historic Downtown



Historic  
Downtown  
Visitor Info  
Centre

SHREVE  
SKATEBOARD  
SNOWBOARD  
APPAREL

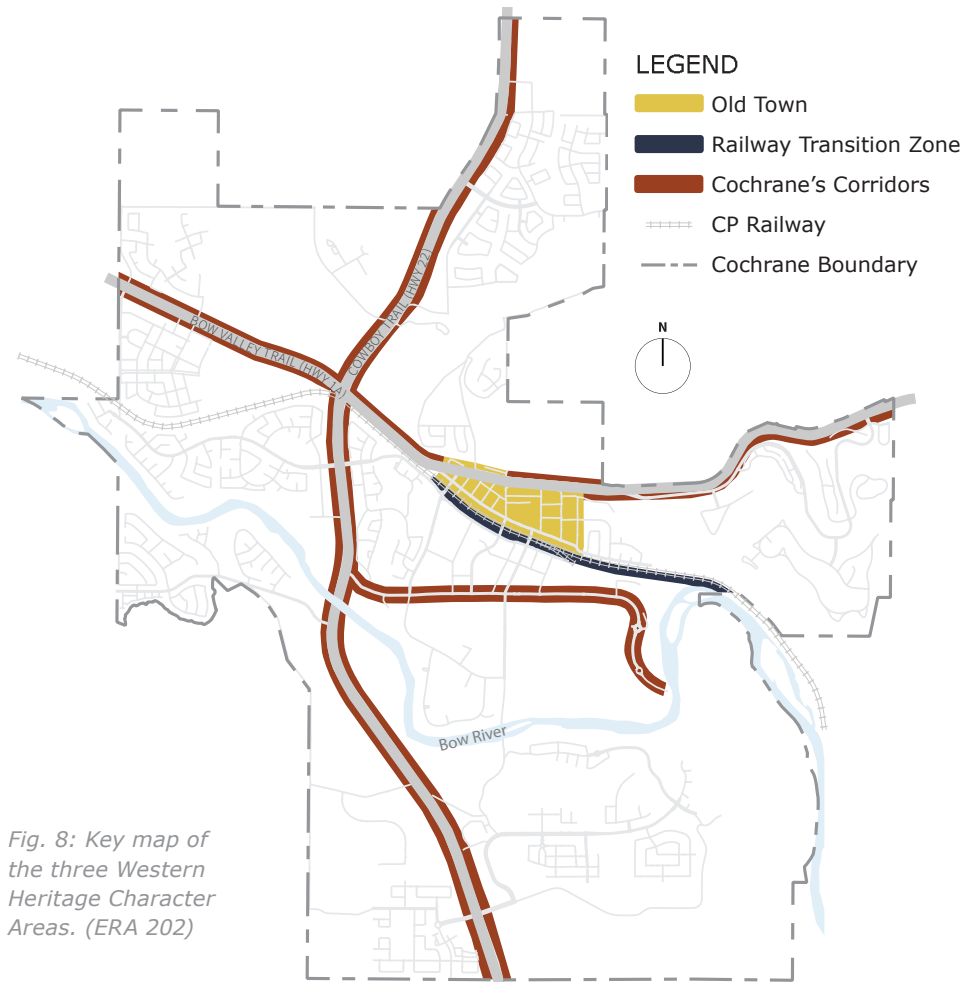
Fig. 7: ERA 2020

# 3.0 WESTERN HERITAGE CHARACTER AREAS

The Western Heritage Design Framework identifies three Character Areas where Western Heritage will be prioritized in the town of Cochrane.

- Character Area 1: The Old Town
- Character Area 2: The Railway Transition Zone
- Character Area 3: Cochrane’s Corridors

The two core elements of Western Heritage (Cochrane’s History, and Small-Town Character) will be expressed in different ways in each of the three Character Areas.



*Fig. 8: Key map of the three Western Heritage Character Areas. (ERA 202)*

### 3.1 The Old Town

The Old Town is Cochrane’s original town site, based on the location of the historic settlement in the late 19<sup>th</sup>-early 20<sup>th</sup> century. It is intended as the priority zone for the celebration of Cochrane’s Western Heritage for locals and visitors.

The Old Town is a triangular area located between the rail corridor (south), the east side of Baird Avenue (east), and the north sides of Second Street West, Third Street West and Highway 1A (north). A map is included on the following page.

The use of the Western Heritage design approach in the Old Town will result in:

- a. the conservation of **physical historic features** like buildings;
- b. the interpretation of **lost historic features** and **intangible historic themes**;
- c. the enhancement of **small-town urban character** through building design and public realm design.

For a large-scale detailed map of the Old Town, see Schedule A.

For Objectives and Guidelines for development projects in the Old Town, see Section 4.1.



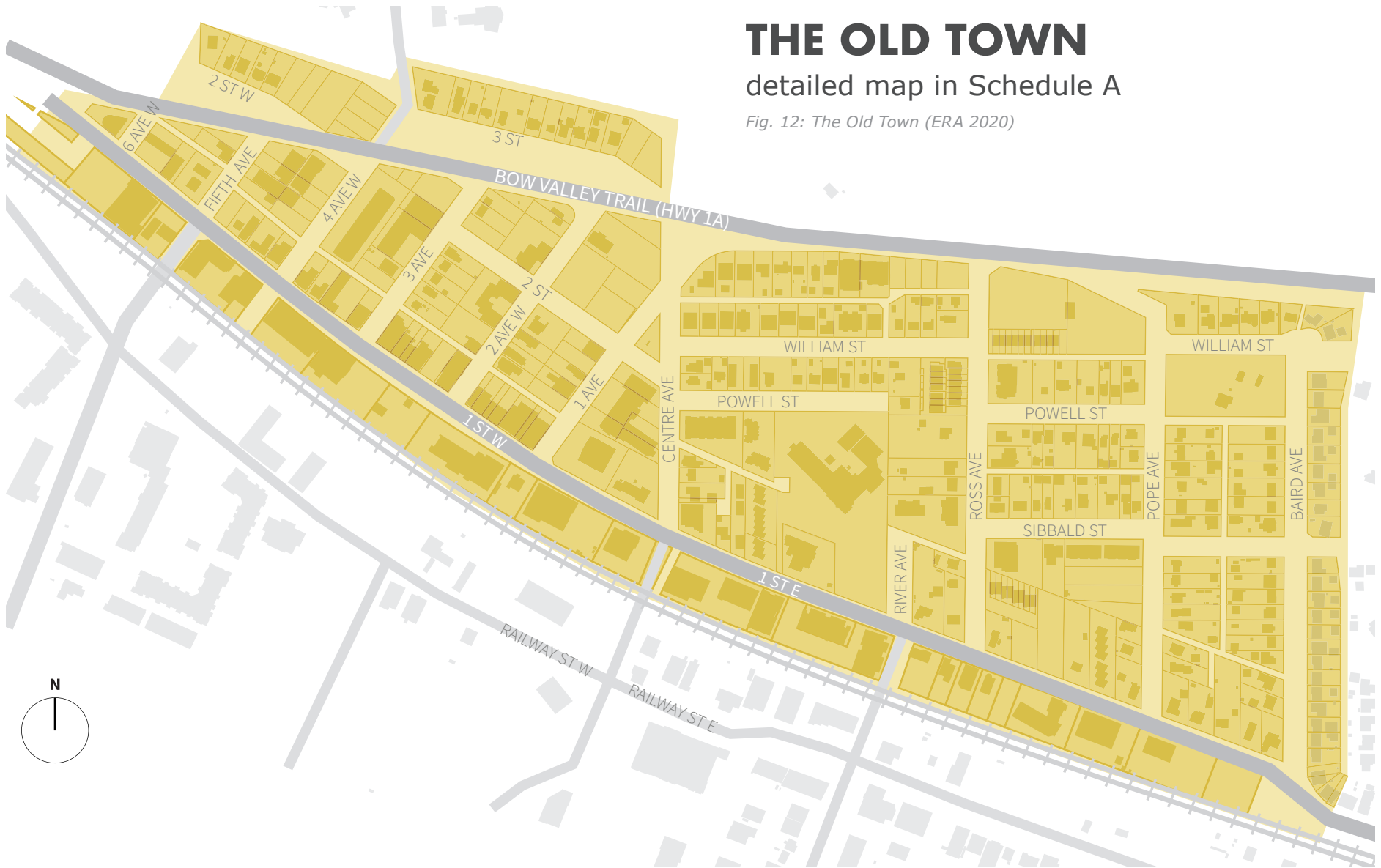
*Figs. 9-11: Snapshots of the Old Town (ERA 2020)*



# THE OLD TOWN

detailed map in Schedule A

*Fig. 12: The Old Town (ERA 2020)*



### 3.2 The Railway Transition Zone

The Railway Transition Zone borders the Canadian Pacific Rail corridor on its south side, immediately south of the Old Town. It is intended as a place to celebrate the railway as a key historic character-defining element in Cochrane; and to transition from the small-scale urban Old Town to the larger-scale contemporary character south of the railway.

The Railway Transition Zone includes all development lots located adjacent to the railway right-of-way (south side) between the rail corridor (north), Railway Street/Glenbow Drive/Griffin Road (south), Glenbrook Road (west) and the Cochrane town limit (east). A map is included on the following page.

The use of the Western Heritage design approach in the Railway Transition Zone will result in:

- a. the conservation and celebration of **the presence of the railway** as a character-defining element of Cochrane;
- b. the interpretation of **lost historic features** and **intangible historic themes**;
- c. the expansion of **urban character, at a larger scale than the Old Town**, to provide a transition to the contemporary urban and suburban character south of Cochrane’s Old Town.

For a large-scale detailed map of the Railway Transition Zone, see Schedule B. For Objectives and Guidelines for development projects in the Railway Transition Zone, see Section 4.2.

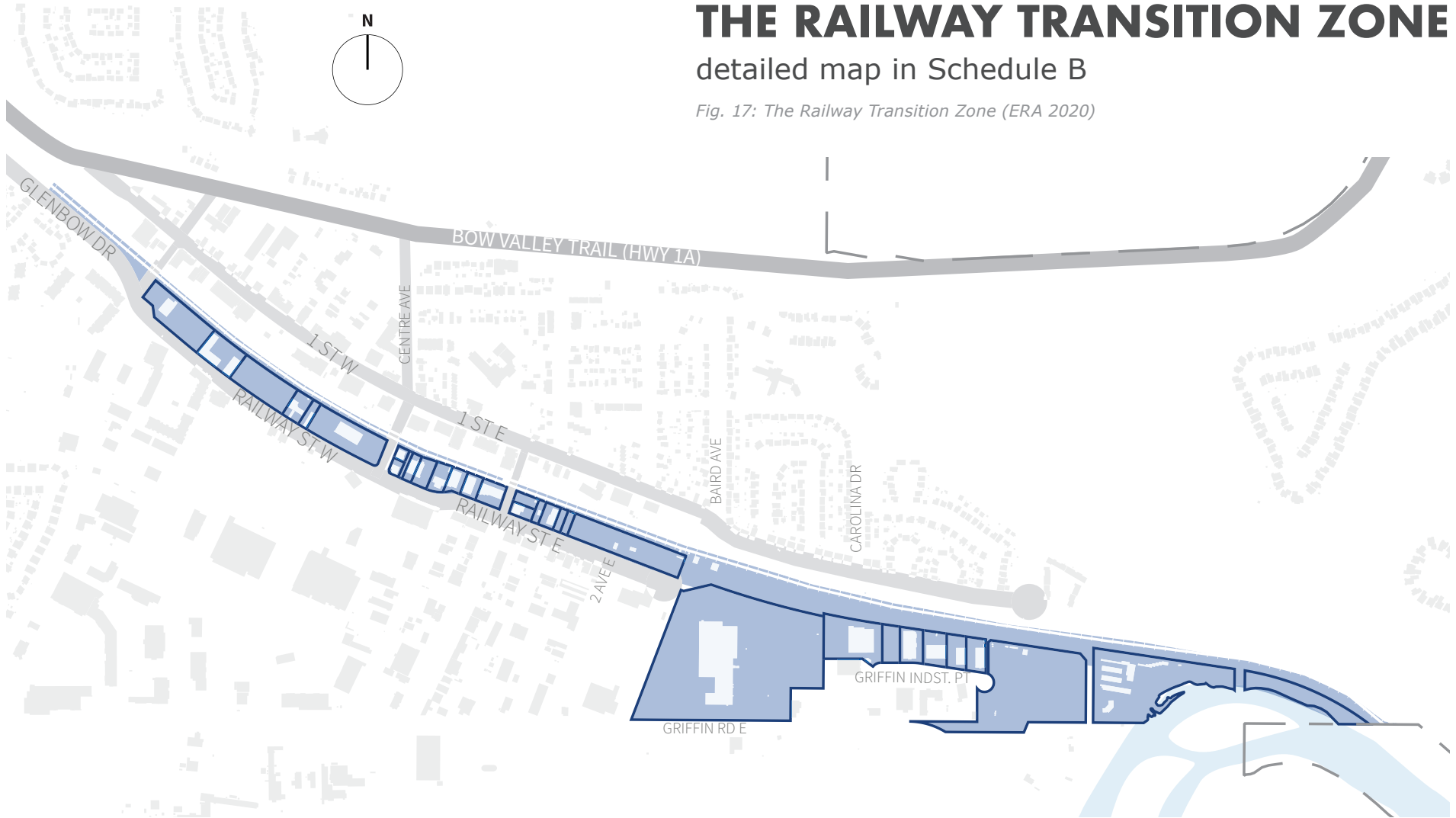


*Figs. 13-16: Snapshots of the Railway Transition Zone. (ERA 2020)*

# THE RAILWAY TRANSITION ZONE

detailed map in Schedule B

*Fig. 17: The Railway Transition Zone (ERA 2020)*



### 3.3 Cochrane's Corridors

The Cochrane's Corridors zone consists of properties bordering Highway 1A (Bow Valley Trail), Highway 22 (Cowboy Trail), and Griffin Road.

It is intended as a zone to communicate Cochrane's unique identity to visitors and commuters passing through town, to identify Cochrane as an engaging place to spend time, and to draw potential visitors into the Old Town.

The Cochrane's Corridors zone includes all lots located adjacent to Highway 1A, Highway 22 and Griffin Road, that are not located within the Old Town or Railway Transition Zone boundaries. These lots are shown in the map on the following page highlighted in rust.

Along Cochrane's Corridors, the Western Heritage design approach is intended to facilitate the communication of Cochrane's identity through the **interpretation of lost historic features and intangible historic themes.**

For large-scale detailed maps of Cochrane's Corridors, see Schedule C.

For Objectives and Guidelines for development projects along Cochrane's Corridors, see Section 4.3.

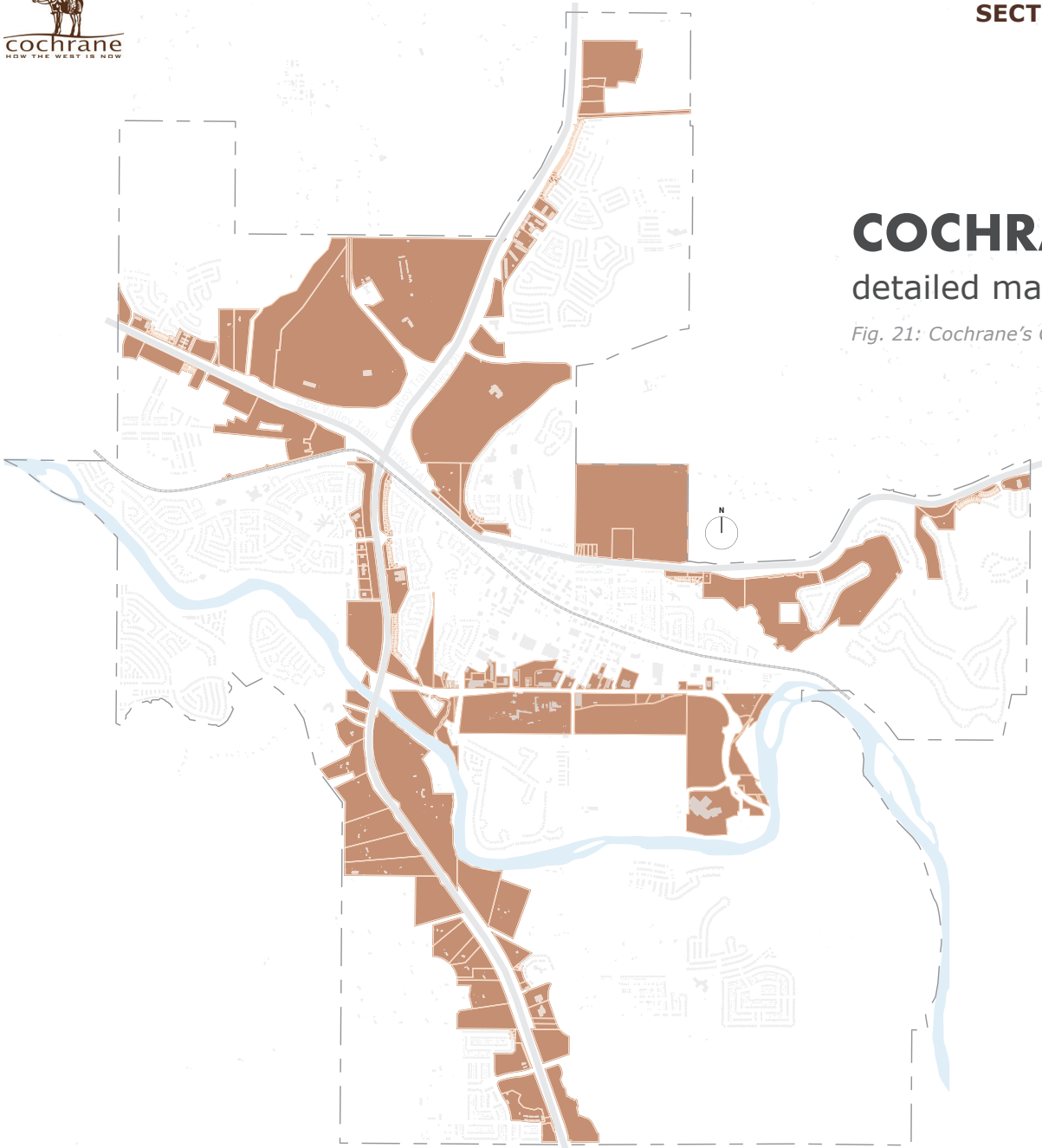


*Figs. 18-20: Snapshots of Cochrane's Corridors. (ERA 2020)*

## **COCHRANE'S CORRIDORS**

detailed maps in Schedule C

*Fig. 21: Cochrane's Corridors (ERA 2020)*



*Note: The Cochrane's Corridors map should be revised in the future as large lots adjacent to these roads are subdivided.*

*In such cases, the subdivided lots that remain adjacent to Highway 1A, Highway 22 or Griffin Road will remain in the Cochrane's Corridors zone, while subdivided lots that are no longer adjacent to any of these three roads will be removed from the zone.*



ROCKYVIEW  
HOTEL

SALOON

STEAKHOUSE

ROOMS

TEXAS CITY GRILL

TEXAS CITY GRILL

504

Fig. 22: ERA 2020

## 4.0 DESIGN OBJECTIVES & GUIDELINES

The Western Heritage Design Framework includes a series of Design Objectives for each of the three Character Areas outlined in Section 3.0.

The Design Objectives offer direction for the expression of Western Heritage on public and private lands in each Character Area.

The **Design Objectives are mandatory** and must be achieved by all non-residential and mixed-use (residential-commercial) development projects in these Character Areas.

Through a Design Brief submitted with a development application, proponents and their consultants will be expected to demonstrate clearly how their proposal meets the set of Design Objectives applicable to their site.

Development projects include:

- New construction
- Additions on existing buildings
- Alterations to existing buildings
- Public works
- Public spaces

**There may be many ways to meet a given Design Objective.** The Town of Cochrane will give due consideration to proponents' creative strategies in achieving the applicable Design Objectives.

In Section 4.0, the Design Objectives are accompanied by supplementary Design Guidelines, which are provided to assist proponents in understanding how to meet the Objectives. **The Guidelines are intended to be assistive, and are not mandatory.**

It may be helpful to engage an architect or designer to develop a strategy to meet the applicable Objectives on a given site.



Fig. 23: ERA 2020

## DESIGN OBJECTIVES: THE OLD TOWN

*Development projects on private properties are expected to meet the following Objectives in the Old Town. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

*The Town of Cochrane is expected to meet the following Objectives for projects undertaken in public spaces throughout the Old Town. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

### **For Development Projects on Private Properties**

### **For Projects in Public Spaces**

- 4.1.1 Development projects shall make every effort to retain and conserve any buildings on Cochrane’s Heritage Register that exist on site.
- 4.1.2 New construction shall be designed to be compatible with, and responsive to, any adjacent and nearby buildings on Cochrane’s Heritage Register.
- 4.1.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.
- 4.1.4 New construction shall contribute to the variation in small-scale, close-knit building forms that conveys the quality of Cochrane’s historic small-town character.
- 4.1.5 New construction on the south side of First Street shall conserve the visual and physical presence of the rail corridor from the public realm.

- 4.1.6 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s heritage in the Old Town, and along First Street.
- 4.1.7 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Old Town, and along First Street.
- 4.1.8 Public works along the south side of First Street shall conserve the visual and physical presence of the rail corridor from the public realm.



### 4.1.1 Development projects shall make every effort to retain and conserve any buildings on Cochrane’s Heritage Register that exist on site.

- a. Buildings identified on the Town of Cochrane’s Heritage Register are encouraged to be adapted for new uses if their existing uses are no longer viable.
- b. Renovations to buildings on the Heritage Register are encouraged to use archival photographs and other physical and documentary evidence to identify the building’s earlier condition and select appropriate materials for restoration.

Where no documentary evidence is available, it is acceptable to make educated estimates based on historical evidence of the context, and an understanding of the building’s architectural era and style.



**4.1.1(a)** Heritage Register buildings are encouraged to be retained and adapted for new uses if their existing uses are no longer viable. When private residences are adapted with public-facing uses, more people are able to share in Cochrane’s built heritage (Fig. 24: ERA 2020)

#### What is Conservation?

Conservation encompasses a range of strategies or treatments to retain (and enhance) historic places’ heritage value and to extend their physical life.

The *Standards and Guidelines for the Conservation of Historic Places in Canada* recognize three main treatments: preservation, rehabilitation and restoration. One of these approaches usually dominates in a conservation project, although aspects of each may play a role.

(from *Managing Historic Places* manual, Province of Alberta)

**Preservation:** protecting, maintaining, and/or stabilizing the existing materials, form, and integrity of a historic place or of an individual component, while protecting its heritage value.

**Restoration:** accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value.

**Rehabilitation:** making possible a continuing or compatible contemporary use of a historic place or an individual component, while protecting its heritage value.

(from the *Standards and Guidelines for the Conservation of Historic Places in Canada*)



**4.1.1(b)** Use archival photos and other physical and documentary evidence to select appropriate materials for restoration. (Fig. 25: Glenbow Archives, PA-2807-4140)

**4.1.1 Development projects shall make every effort to retain and conserve any buildings on Cochrane’s Heritage Register that exist on site.**

- c. Renovations to buildings on the Heritage Register are encouraged to accurately restore earlier features and conditions if possible. Building materials (brick, windows, ornamental features) should be replaced in kind.
- d. Renovations to buildings in the Old Town should not mimic historical architectural styles, as this may create a false sense of Cochrane’s historical development and may detract from the value of authentic historic resources. Facade improvements that have been made to mimic historical architectural styles should be reversed where possible.
- e. Alterations, additions and facade improvements to properties on the Heritage Register are encouraged to consult the *Standards and Guidelines for the Conservation of Historic Places in Canada (2010)* for guidance on appropriate techniques in restoration, rehabilitation and reuse.
- f. Additions to buildings on the Heritage Register should be distinguishable as contemporary construction to conserve the integrity of Cochrane’s early building stock.



**4.1.1(b)** It is recommended to use archival photos to identify and restore a building’s earlier condition. Renovations could aim to restore the building’s original condition, or a later condition that is also considered to convey the property’s heritage value. (Figs. 26-28, clockwise from left: Glenbow Archives NA-966-1 / PA-1365-1 / NC-26-206)



**4.1.1(c)** The peach-tinted buff brick from local brickyards became a common early building material in Cochrane and the region. Original brick should be salvaged wherever possible to provide for restorations to other brick buildings using matching materials. (Fig. 29: ERA 2020)

### 4.1.2 New construction shall be designed to be compatible with, and responsive to, any adjacent and nearby buildings on Cochrane’s Heritage Register.

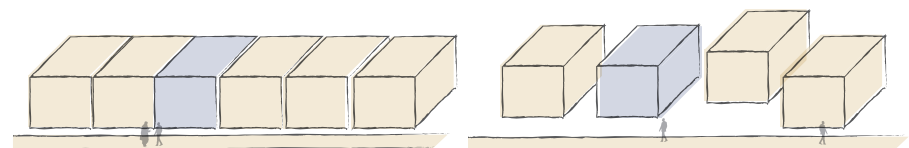
- a. New buildings should be constructed at a scale designed to appear consistent with any adjacent or nearby buildings on Cochrane’s Heritage Register (including commercial, residential, institutional or other Heritage Register buildings).
- Taller buildings can use strategies to minimize the appearance of their height, like upper-storey stepbacks, or distinct changes in material beyond the existing streetwall height.
  - Wider buildings can use strategies to break up their massing, like the use of vertical columns or piers to articulate storefronts at the ground floor, or a change in materials at regular intervals.
- b. New construction near buildings on Cochrane’s Heritage Register should use the adjacent context as guidance for front and side setbacks:
- Where there is a continuous street wall, new buildings should continue the street wall.
  - Where there are varying front setbacks and/or spaces between buildings, new construction should be consistent with this pattern.
  - Avoid using existing strip malls as a guide for front setbacks, even if they are located in the property’s immediate context.



**4.1.1(a)** Upper-storey stepbacks used to accommodate additional height while fitting in with the older building next door (Fig. 30: 3144 Architects, 2019)



**4.1.1(a)** Vertical columns and pedestrian-scale overhangs to convey small storefronts at ground level, and an upper-storey stepback used to accommodate additional height (Fig. 31: Arch11 2020)



**4.1.2(b)** New buildings (in blue) designed and sited to fit in with the existing context of front and side setbacks (Fig. 32: ERA 2020)

**4.1.2 New construction shall be designed to be compatible with, and responsive to, any adjacent and nearby buildings on Cochrane’s Heritage Register.**

- c. New buildings should be distinguishable as contemporary construction to conserve the integrity of Cochrane’s early building stock. They should not mimic historical architectural styles. They should avoid creating a false sense of historical development.
- d. New buildings should use creative strategies to be compatible with adjacent or nearby buildings on Cochrane’s Heritage Register. Example strategies include:
- A colour or material palette that includes wood or buff (yellow) brick;
  - An upper-storey window pattern that responds to the adjacent ratio of solids-to-voids;
  - Datum lines that contribute to consistency or slight variation along the street wall;
  - Reference to the existing rhythm of bays and entrances along the street;
  - Contemporary interpretations of turn-of-the-20<sup>th</sup>-century building shapes and forms (including flat and pitched roofs).



**4.1.2(d)** The traditional-style building sketch on the right successfully responds to the adjacent Heritage Register building using (1) buff brick cladding; (2) punched windows of a similar scale and orientation; (3) consistent datum lines; (4) retail bays of a similar width; and (5) a flat roof. (Fig. 33: ERA 2020)



**4.1.2(d)** The contemporary-style building sketch on the right successfully responds to the adjacent Heritage Register building using (1) buff brick cladding; (3) consistent datum lines; and (5) a flat roof. (Fig. 34: ERA, 2020)

**4.1.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.**

- a. Buildings should convey the appearance of heights of 1-3 storeys.

If taller buildings or upper-storey additions are permitted under the land-use bylaw, they should use strategies to minimize the appearance of their height, like upper-storey stepbacks, or distinct changes in material above the existing streetwall height.

- b. Buildings should be constructed with little-to-no setback from the **front** property line.

Use the adjacent and nearby building pattern to determine whether a zero-lot-line setback is appropriate, or whether a minimal setback is more consistent with the existing streetscape. Larger setbacks may be appropriate to accommodate a street-facing porch or patio.

- c. Any on-site parking should be located at the rear of the property. On-site parking lots should incorporate landscaped buffers and prominent pedestrian connections to the public sidewalk.

- d. It is encouraged to retrofit properties with large setbacks or front-yard parking (e.g. strip malls) by relocating parking to the rear of the property, and/or to converting and animating front-yard parking lots with active uses. Examples could include patios, pop-up markets, or winter skating surfaces.



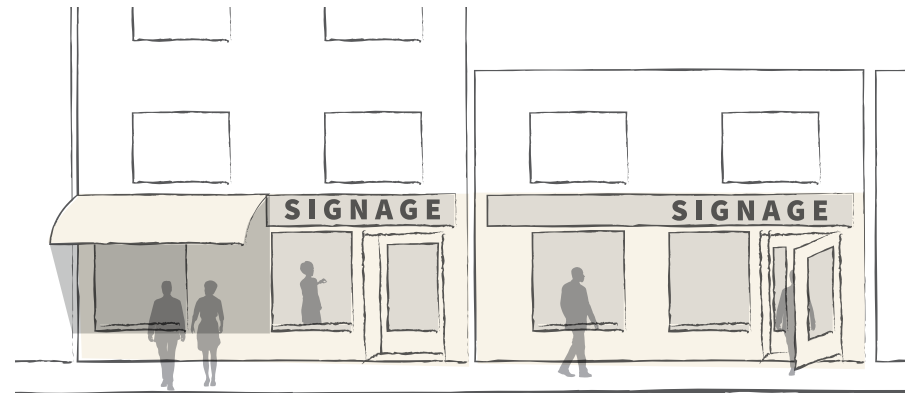
**4.1.3(c)** Locate on-site parking lots at the rear, with landscaped buffers and safe pedestrian connections back to public sidewalks (Fig. 35: ERA 2020)



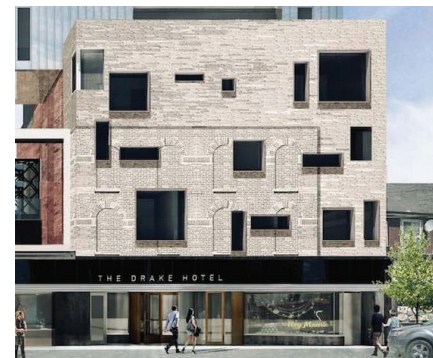
**4.1.3(d)** Strip-mall parking retrofitted with public amenities and animated with public uses (Fig. 36: Cait Kramer, 2019)

### 4.1.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.

- e. Buildings are encouraged to locate public-facing uses at ground level. These might include retail businesses, restaurants or professional services.
- f. New buildings should be designed with storefronts at ground level. They should incorporate visual interest through functional entrances, large permeable glazing to display interior wares and/or activities, signage positioned at a pedestrian scale, and engaging outdoor features like menus, flower baskets or bench seating.
- g. Buildings with street frontage wider than 20m are encouraged to convey the appearance of multiple smaller storefronts facing the street, e.g. through vertical articulation, small jogs in the facade, changes in materiality.
- h. Consider how subtle texture and visual interest can be incorporated into a new building's principal façade through materiality, openings, permeability, and architectural articulation.
- i. Buildings with multiple frontages onto the public realm (e.g. corner buildings, buildings adjacent to public spaces) should provide visual interest along each elevation, avoiding blank walls, but maintaining a distinct visual hierarchy between the principal façade and secondary public-facing façade.



**4.1.3(f)** Storefront elements include retail entrances, permeable glazing, and signage positioned at heights designed for pedestrians to read (Fig. 37: ERA 2020)



**4.1.3(h)** Consider how contemporary design elements can be used to achieve visual interest. Here, irregular contemporary windows are juxtaposed with brick imprints of earlier historic window forms. (Fig. 38: Diamond Schmitt 2020)



**4.1.3(f)** Consider the use of permeable garage-style doors, which can be opened in warm weather to allow interior activity to spill into the public realm. (Fig. 39: Park Distillery)

**4.1.4 New construction shall contribute to the variation in small-scale, close-knit building forms that conveys the quality of Cochrane’s historic small-town character.**

- a. Buildings should convey the appearance of heights of 1-3 storeys.

Where taller buildings or upper-storey additions are permitted under the land use bylaw, they should use strategies to reference a 1-3 storey building form, like an upper-storey stepback, or a distinct change in material beyond the existing streetwall height.

- b. Buildings should be constructed with little-to-no setback from the **front** property line.

Use the adjacent and nearby building pattern to determine whether a zero-lot-line setback is appropriate, or whether a minimal setback is more consistent with the existing streetscape. Larger setbacks may be appropriate to accommodate a street-facing porch or patio.

- c. Buildings should be constructed with little-to-no setback from the **side** property lines.

Use the adjacent and nearby building pattern to determine whether a zero-lot-line side setback is appropriate, e.g. if there is already a continuous streetwall. Minimal side setbacks may be appropriate where there are adjacent or nearby house-form buildings.



**4.1.4(a)** A distinct material change is one way to convey the appearance of a smaller-scale building at the streetwall (Fig. 40: *Globe and Mail*, 2018)



**4.1.4(a)** An upper-storey stepback is one way to convey the appearance of a smaller-scale building at the streetwall (Fig. 41: *HOLST Architects*, 2006)



**4.1.4(b-c)** The streetwall along the 200-300 blocks of First St W, like many historic commercial streets, consists of buildings built up to the front and side property lines, with no spaces between buildings. (Fig. 42: *ERA*, 2020)



**4.1.4(b-c)** The rest of the historic town consists of former residential lots converted for commercial uses. In these areas, the dominant streetwall consists of buildings with small front lawns, and small spaces between buildings. (Fig. 43: *ERA*, 2020)

**4.1.4 New construction shall contribute to the variation in small-scale, close-knit building forms that conveys the quality of Cochrane’s historic small-town character.**

- d. New buildings should choose from the variety of roof forms found in the Old Town, including flat roofs and pitched roofs.

Contemporary interpretations of false-front parapets on pitched-roof buildings may be acceptable if they are distinguishable as new construction and do not create a false sense of historical development.

- e. New construction should not seek to replicate the appearance of any existing building or facade within the Old Town, unless the proposal represents a contemporary interpretation of an historic building facade.



**4.1.4(d)** A false-front parapet was an historic method of converting a residential building (with a pitched roof) into a commercial storefront. The false front was meant to make the building appear as though it had a flat roof. (Fig. 44: Glenbow Archives, PA-1599-573-107)



**4.1.4(d)** Consider the history of diverse roof pitches and styles in Cochrane’s Old Town, seen here from the northwest in 1910, when designing a new building. (Fig. 45: Cochrane Historical and Archival Preservation Society)



**4.1.4(e)** An imprinted screen on the upper floors of a contemporary building references the facade of an earlier building on site, since lost (Fig. 46: Quadrangle, 2016)



### 4.1.5 New construction on the south side of First Street shall conserve the visual and physical presence of the rail corridor from the public realm.

- a. New construction on the south side of First Street is encouraged to frame or provide glimpses to the rail corridor.

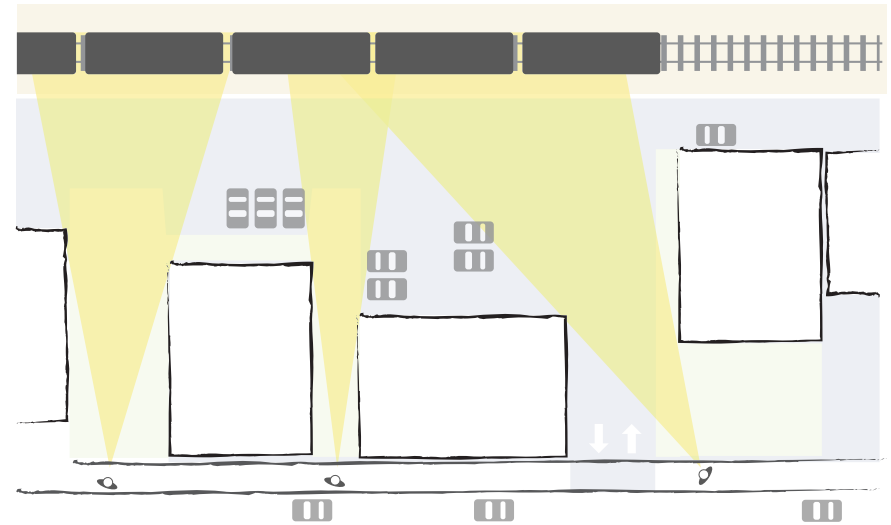
This could be achieved through creative design approaches to:

- building setbacks;
- orientation;
- form;
- massing; and
- glazing for transparency.

- b. Safety barriers to the rail corridor are encouraged to be upgraded with creative aesthetic treatments, e.g. framing or highlighting views to the rail corridor, or using materials that interpret the district’s rail heritage (wood, weathering steel).

- c. Wherever possible within existing safety requirements and other constraints, privately-owned parking lots and open spaces adjacent to the railway should be retrofitted and landscaped as public gathering places.

The retrofitted spaces should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.



**4.1.5(a)** When designing new buildings, consider how their placement, orientation, and design could frame or highlight views to the rail corridor. (Fig. 47: ERA 2020)



**4.1.5(b)** Upgrade barriers with creative aesthetic treatments to frame, highlight and celebrate rail corridor views. (Fig. 48: ERA 2020)

**4.1.5 New construction on the south side of First Street shall conserve the visual and physical presence of the rail corridor from the public realm.**



**4.1.5(a)** This sketched building has been sited and designed to offer framed views of the rail corridor. It also supports the character of the Railway Transition Zone through cladding materials that interpret the railway's industrial character. (Fig. 49: ERA 2020)

**4.1.6 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s heritage in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**

- a. The Town of Cochrane should explore a Heritage Interpretation Fund (Section 1.4) to deliver and facilitate interpretation in the Old Town, and elsewhere in Cochrane.
- b. The Town of Cochrane will engage with diverse stakeholders to develop a Heritage Thematic Framework, which will synthesize the themes of valued cultural heritage in Cochrane.

Future Town-led interpretation should rely on both the Thematic Framework and site-specific studies to determine the most appropriate histories for interpretation on a given site.

- c. The Town of Cochrane should employ diverse media for interpretation within the public realm. These could include:

- Artifact displays
- Audio/video clips
- Images/photographs
- Interpretive design of public-realm elements
- Plaques/panels
- Public art
- Public programming: tours, events, festivals
- Smartphone apps
- Websites

**What is Interpretation?**

Heritage Interpretation refers to the range of strategies (installations, media, activities) that may be used to enhance the public’s awareness and understanding of an historic place. Interpretation allows us to communicate histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

**What’s a Thematic Framework?**

A Heritage Thematic Framework is an historical analysis that synthesizes the valued themes of a place’s history. It is developed through historical research and analysis, and meaningful public engagement. It can be used as a tool to ensure that conservation decisions and the development of interpretive media.



**4.1.6(c)** Employ diverse media, like large-scale panel installations, to interpret Cochrane’s histories within the public realm (Fig. 50: Cloud Gehshan)

**4.1.6 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s heritage in the Old Town, and along First Street.**

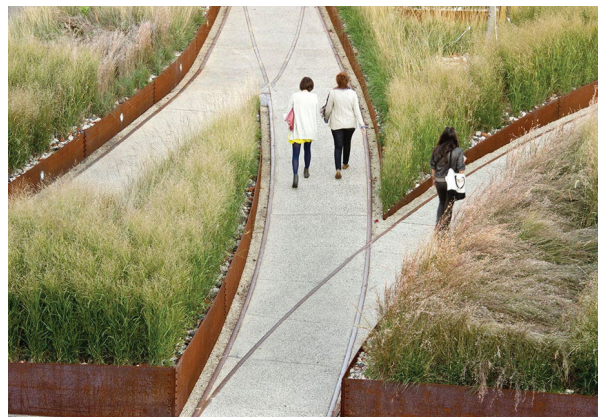
**OBJECTIVE FOR PUBLIC SPACES**



**4.1.6(c)** A diversity of interpretive **public art installations**: standing sculptural pieces, murals, poetry embedded in the sidewalk. (Figs. 51-55, clockwise from top left: Spencer Purdy / J. F. Manzo, 2009 / Town of Cochrane, 2016 / ERA, 2017 / CODAWORX, 2015 )

**4.1.6 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s heritage in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**



**4.1.6(c) Interpretive building and landscape design:** public buildings and spaces can be designed with the intent of interpreting key themes in Cochrane’s history. (Figs. 56-60, clockwise from top left: Yellow Camera / J. J. Tiziou, Landezine / S9 Architecture / Yellow Camera / Yellow Camera )

**OBJECTIVE FOR PUBLIC SPACES**

**4.1.6 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s heritage in the Old Town, and along First Street.**

- d. The Town of Cochrane should recognize heritage interpretation as an opportunity to convey the histories that are no longer apparent in the town’s built form, including the traditional practices of First Nations, and the histories of important sites or institutions that are no longer standing in town.
- e. The Town of Cochrane should recognize events and programs as methods for heritage interpretation. These could range from historical walking tours to contemporary events and festivals that serve to animate and draw visitors to the Old Town.
- f. The Town of Cochrane should capitalize on public spaces and pedestrian trails as key locations for interpretive installations. Consider not only standing installations (panels, sculptures, murals) but public art works inlayed into the ground and/or an interpretive design for the public spaces or trails themselves (e.g. route, landscape materials).
- g. The Town of Cochrane is encouraged to partner with diverse stakeholders to implement and deliver interpretation in the public realm. Relevant stakeholder groups may include First Nations groups, museums, libraries and archives, local business owners, local artists, community organizations, and property owners and developers.

- h. The Town of Cochrane is strongly encouraged to partner with local First Nations to develop an in-depth understanding of the valued cultural heritage associated with the town site and the broader region.



**4.1.6(f)** Capitalize on public spaces and pedestrian trails as key locations for interpretive installations, like panels (left) and interactive art works (right). (Figs. 61-62, from left to right: Entro / Martin Reis)

**4.1.7 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**

a. The Town of Cochrane should implement key elements to support walkability on new and existing streets, including:

- Sidewalks on both sides of the street
- Small-block street grids
- Regularly-spaced, well-marked pedestrian crosswalks at all intersections, and mid-block crosswalks on larger blocks
- Pedestrian-scale street lighting
- Vehicle-traffic calming measures like curb bumpouts, street trees and crosswalks enhanced with paint or pavers
- Landscaped buffers between sidewalks and the roadway
- Street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding)

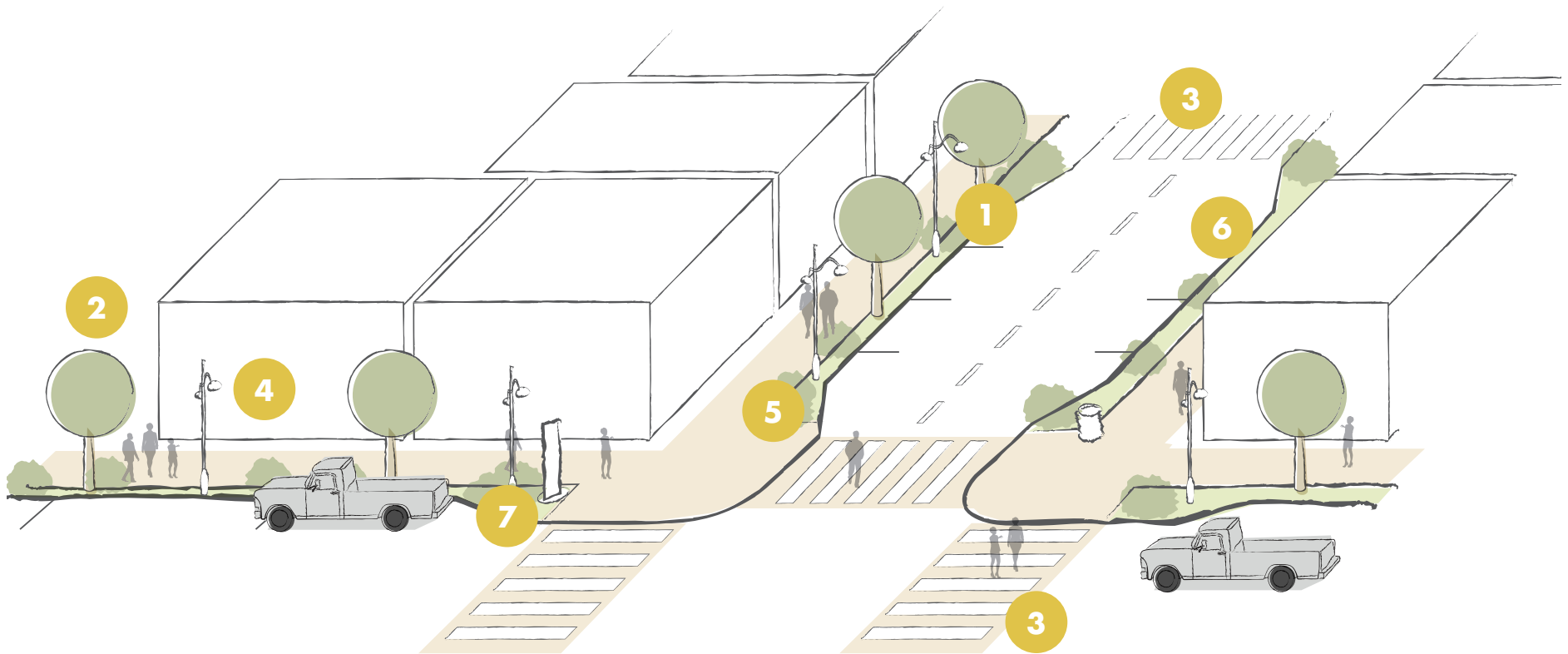
b. The Town of Cochrane is encouraged to consider enhanced public-realm strategies for First Street, designed to mark it as a central gathering place for locals and visitors. These could include a distinct roadway paving treatment (e.g. brick pavers) or a woonerf approach.



**4.1.7(b)** Consider enhanced public realm strategies -temporary or permanent- for First Street to mark it as a central gathering place (Figs. 63-64, from top to bottom: Nice Right Now / Activate YYC)

**4.1.7 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**



**4.1.7(a)** New and existing streets should support walkability through: (1) sidewalks on both sides; (2) small-block street grids; (3) regular and well-marked pedestrian crosswalks; (4) pedestrian-scale street lighting; (5) traffic calming measures like curb bumpouts, street trees, and painted crosswalks; (6) landscaped buffers between sidewalks and the roadway; and (7) street furniture and amenities. (Fig. 65: ERA 2020)



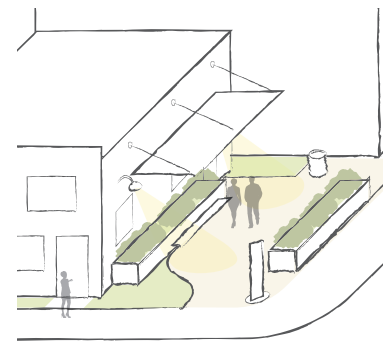
**4.1.7 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**

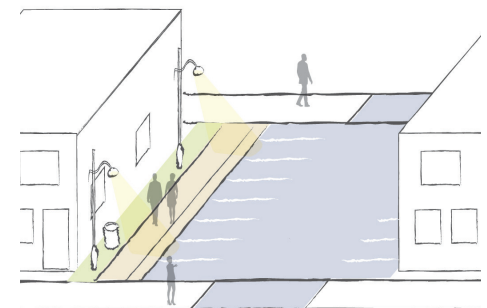
- c. The Town of Cochrane is encouraged to continue to develop and implement a cohesive design language for street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding).
- d. The Town of Cochrane should build on its existing street art program with a town-wide signage/wayfinding strategy that recognizes and celebrates Cochrane’s Old Town site as a gathering place for locals and visitors, with:
  - Gateway markers;
  - Interpretive design that references Cochrane’s heritage through imagery and materiality.
- e. Public spaces should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.
- f. The Town of Cochrane is encouraged to provide street parking throughout the Old Town.
- g. Publicly-owned street-fronting parking lots are encouraged to be retrofitted with clearly-marked pedestrian paths and connections to public sidewalks. These paths should be equipped with pedestrian-scale lighting to facilitate their use at all hours.



**4.1.7(d)** Build on the success of the existing Street Art panels and banners, and consider how they can be scaled across Cochrane while prioritizing the Old Town as a Cochrane’s key gathering place. (Figs. 66-67: Town of Cochrane / ERA 2020)



**4.1.7(e)** Equip public spaces with street furniture, amenities, electrical connections, all-season design. (Fig. 68: ERA, 2020)



**4.1.7(g)** Retrofit parking lots with clearly-marked, well-lit pedestrian paths, with safe connections to public sidewalks (Fig. 69: ERA, 2020)

**4.1.7 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Old Town, and along First Street.**

**OBJECTIVE FOR PUBLIC SPACES**

- h. The Town of Cochrane is encouraged to incentivize and partner with private property owners, businesses and organizations to animate surface parking lots with installations or community events (e.g. pop-up markets, winter skating surfaces).



**4.1.7(h)** Consider how public programming like a pop-up installation can be used to animate gaps (e.g. parking lots) in the Old Town’s streetwall. (Fig. 70: ERA, 2020)



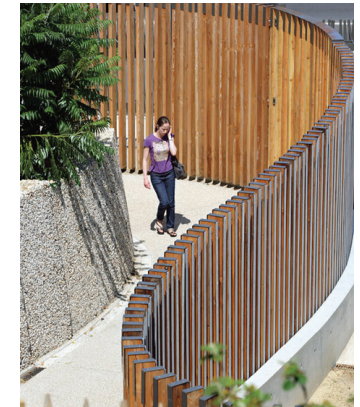
**4.1.7(h)** Consider how public programming like a farmer’s market can be used to animate gaps (e.g. parking lots) in the Old Town’s streetwall. (Fig. 71: Town of Cochrane)

**4.1.8 Public works along the south side of First Street shall conserve the visual and physical presence of the rail corridor from the public realm.**

**OBJECTIVE FOR PUBLIC SPACES**

- a. Any development or alteration of public spaces on the south side of First Street should conserve expansive views of the rail corridor.
- b. Safety barriers to the rail corridor are encouraged to be upgraded with creative aesthetic treatments, e.g. framing or highlighting views to the rail corridor, or using materials that interpret Cochrane’s rail heritage (wood, weathering steel).
- c. Wherever possible within existing safety requirements and other constraints, publicly- owned parking lots and open spaces on the south side of First Street should be retrofitted and landscaped as public gathering places.

They should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.



**4.1.8(b)** Upgrade barriers with creative aesthetic treatments to frame, highlight and celebrate rail corridor views. (Figs. 72-73, from left to right: ERA 2020 / Agence Territoires)



**4.1.8(c)** Retrofitting parking lots and open spaces with street furniture and electrical connections allows them to be used for temporary activations like food markets, winter light installations, and more. (Fig. 74: Town of Cochrane)



Fig. 75: ERA 2020

## DESIGN OBJECTIVES: RAILWAY TRANSITION ZONE

*Development projects on private properties are expected to meet the following Objectives in the Railway Transition Zone. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

### **For Development Projects on Private Properties**

- 4.2.1 New construction shall conserve the visual and physical presence of the rail corridor from the public realm.
- 4.2.2 New construction shall contribute to the interpretation of Cochrane's rail heritage, through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.
- 4.2.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.

*The Town of Cochrane is expected to meet the following Objectives for projects undertaken in public spaces throughout the Railway Transition Zone. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

### **For Projects in Public Spaces**

- 4.2.4 Public works shall conserve the visual and physical presence of the rail corridor in the Railway Transition Zone, and along Railway Street.
- 4.2.5 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane's rail heritage in the Railway Transition Zone, and along Railway Street.
- 4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along First Street and Railway Street.

### 4.2.1 New construction shall conserve the visual and physical presence of the rail corridor from the public realm.

- a. New construction is encouraged to frame or provide glimpses to the rail corridor.

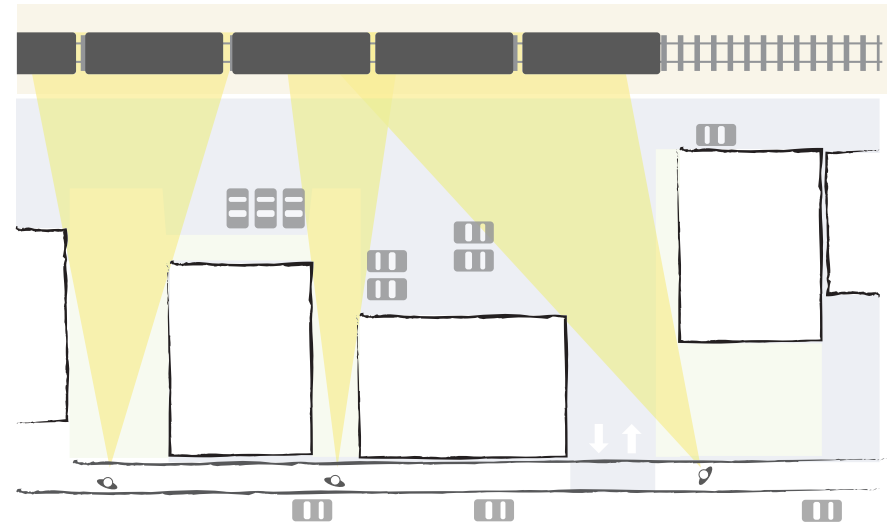
This could be achieved through creative design approaches to:

- building setbacks;
- orientation;
- form;
- massing; and
- glazing for transparency.

- b. Safety barriers to the rail corridor are encouraged to be upgraded with creative aesthetic treatments, e.g. framing or highlighting views to the rail corridor, or using materials that interpret the district's rail heritage (wood, weathering steel).

- c. Wherever possible within existing safety requirements and other constraints, privately-owned parking lots and open spaces adjacent to the railway should be retrofitted and landscaped as public gathering places.

The retrofitted spaces should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.



4.2.1(a) When designing new buildings, consider how their placement, orientation, and design could frame or highlight views to the rail corridor. (Fig. 76: ERA 2020)



4.2.1(b) Upgrade barriers with creative aesthetic treatments to frame, highlight and celebrate rail corridor views. (Fig. 77: ERA 2020)

### 4.2.1 New construction shall conserve the visual and physical presence of the rail corridor from the public realm.



**4.2.1(a)** This sketched building has been sited and designed to offer framed views of the rail corridor. It also supports the character of the Railway Transition Zone through cladding materials that interpret the railway's industrial character. (Fig. 78: ERA 2020)

**4.2.2 New construction shall contribute to the interpretation of Cochrane’s rail heritage, through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.**

- a. A development’s interpretive contribution will be provided through an on-site interpretive installation valued at 2.0% of the project’s construction costs.
- b. Property owners are expected to familiarize themselves with their site’s and area’s history and relevant historic themes, using resources including archaeological studies, site-specific historical records, and others made available by the Town of Cochrane, CHAPS and/or the Stockmen Memorial Library.  
 This is encouraged early in the development process so that interpretation may be holistically integrated into the site or building design where appropriate.
- c. Property owners are encouraged to consider creative interpretation strategies in lieu of typical plaques and panels. These might include:
  - Art (e.g. sculptures, murals)
  - Audio/video clips
  - Images/photographs
  - Interpretive design of buildings or landscape features to evoke themes of the site’s history

**What is Interpretation?**

Heritage Interpretation refers to the range of strategies (installations, media, activities) that may be used to enhance the public’s awareness and understanding of an historic place. Interpretation allows us to communicate histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.



**4.2.1(b)** Engage with CHAPS and other resources (e.g. Glenbow Western Research Centre at the University of Calgary) to learn about Cochrane’s rail heritage, and the building typologies and industries the railway fostered south of the historic town. (Fig. 79: Glenbow Archives, NC-26-209)



### 4.2.2 New construction shall contribute to the interpretation of Cochrane's rail heritage, through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.

- d. On-site interpretation strategies might reference the imagery, materials, forms, motifs and other physical characteristics associated with a site's significant histories and historical themes. Architects, artists and landscape designers are encouraged to consider how these might be subtly integrated into the contemporary development on site.
- e. Property owners are encouraged to engage or partner with local artists (including sculptors, painters, poets, writers, or filmmakers), community organizations, historians and other professionals to develop the interpretive media.

New installations are strongly recommended to embrace understated and/or sophisticated reference to the stories they tell; this is best achieved by engaging a professional artist or designer in their development.

- f. On-site interpretive installations should avoid visual noise and competition with adjacent or nearby installations. Consider nearby interpretive installations early in the design process, and develop a strategy that either complements them, or otherwise does not detract from them and/or overwhelm the streetscape.

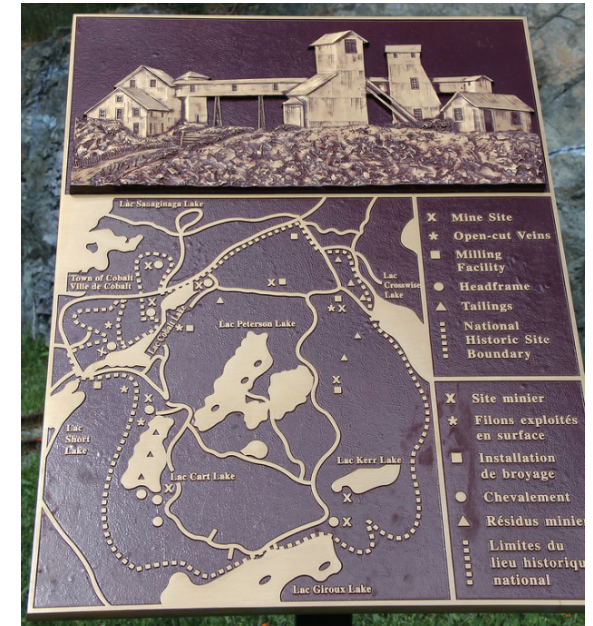
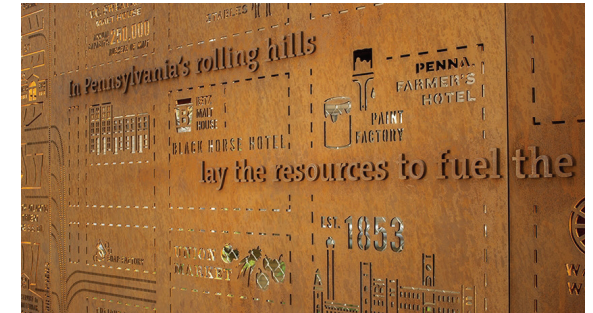
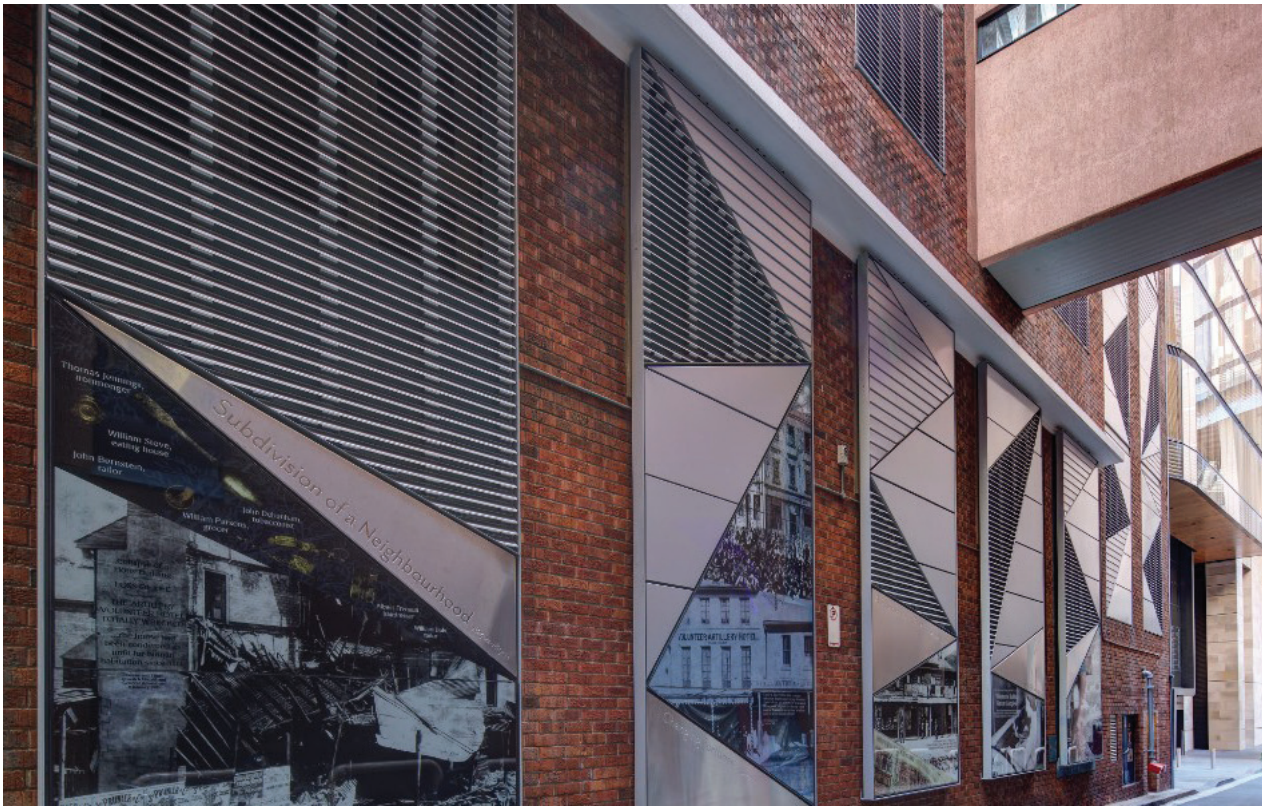


**4.2.2(c)** Buildings (and site landscapes) can be designed to create subtle interpretations of historic forms and themes, e.g. an interpretation of industrial railway-side building forms. (Fig. 80: Robert Lemermeyer, 2019)



**4.2.2(d)** Consider how metals, weathered steel and other industrial materials can be used to evoke rail heritage and celebrate the Railway Transition Zone as an historically industrial district. (Fig. 81: Design Milk 2020)

**4.2.2 New construction shall contribute to the interpretation of Cochrane’s rail heritage, through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.**



**4.2.2(c)** Plaques and panels are the most common form of heritage interpretation, but are not considered to be the most successful method of engaging a diverse range of public audiences. Property owners who choose to commission plaques / panels for their interpretive contribution should incorporate engaging graphics (e.g. maps, photographs) and/or interactive features. (Fig. 82-84, clockwise from top left: GML Heritage / Cloud Gehshan / Wayne Prout)

### **4.2.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.**

- a. Buildings should convey the appearance of heights of 2-4 storeys.

Where taller buildings or upper-storey additions are permitted under the land use bylaw, they should use strategies to reference a 2-4 storey building form, like an upper-storey stepback, or a distinct change in material beyond the existing streetwall height.

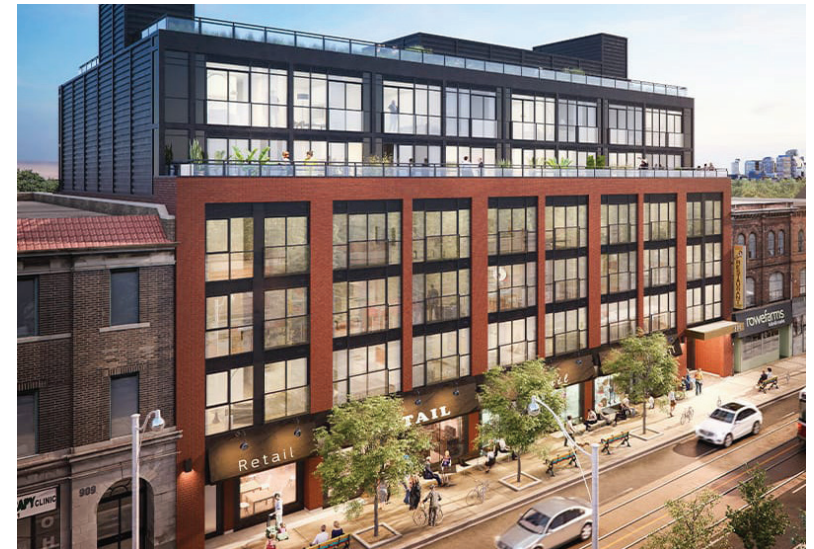
- b. Buildings should be constructed with little-to-no setback from the **front** property line.

Use the adjacent and nearby building pattern to determine whether a zero-lot-line setback is appropriate, or whether a minimal setback is more consistent with the existing streetscape. Larger setbacks may be appropriate to accommodate a street-facing porch or patio.

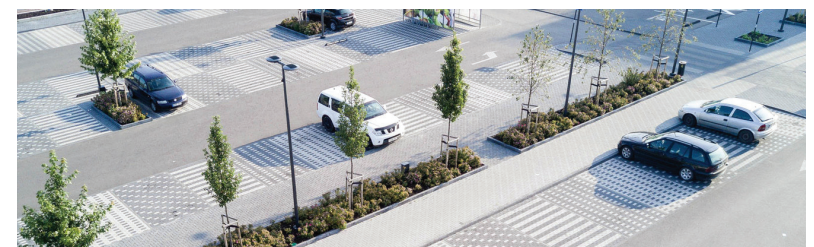
- c. Any on-site parking should be located at the rear of the property. If this is not possible due to proximity to the rail corridor, parking areas may be located along the side of the property, providing sightlines to the rail corridor.

- d. On-site parking lots should incorporate landscaped buffers and prominent pedestrian connections to the public sidewalk.

- e. It is encouraged to retrofit properties with large setbacks or front-yard parking (e.g. strip malls) by relocating parking to the rear of the property, and/or converting and animating front-yard parking lots with active uses. Examples could include patios, pop-up markets, or winter skating surfaces.



**4.2.3(a-b)** Site buildings with little-to-no setback from the front property line, and use strategies like upper-storey stepbacks to minimize heights greater than 4 storeys. (Fig. 85: Daniels Gateway Rental Communities)



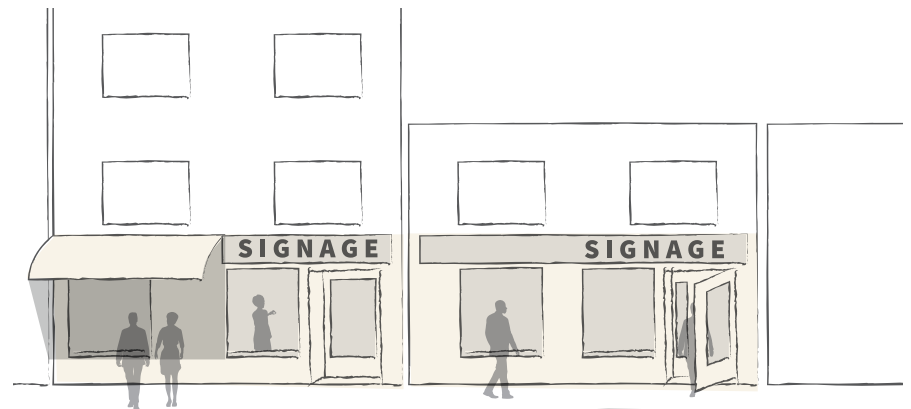
**4.2.3(c-d)** Locate on-site parking lots at the rear, with landscaped buffers and safe pedestrian connections back to public sidewalks (Fig. 86: Norbert Tukaj, Arch Daily)

**4.2.3 New construction shall be designed to contribute to a comfortable and engaging pedestrian experience through pedestrian-scale design and ground-level activation.**

- f. Buildings are encouraged to locate public-facing uses at ground level. These might include retail businesses, restaurants or professional services.
- g. New buildings should be designed with storefronts at ground level. They should incorporate visual interest through functional entrances, large permeable glazing to display interior wares and/or activities, signage positioned at a pedestrian scale, and engaging outdoor features like menus, flower baskets or bench seating.
- h. Buildings with street frontage wider than 20m are encouraged to convey the appearance of multiple smaller storefronts facing the street, e.g. through vertical articulation, small jogs in the facade, changes in materiality.
- i. Consider how subtle texture and visual interest can be incorporated into a new building’s principal façade through materiality, openings, permeability, and architectural articulation.
- j. Buildings with multiple frontages onto the public realm (e.g. corner buildings, buildings adjacent to public spaces) should provide visual interest along each elevation, avoiding blank walls, but maintaining a distinct visual hierarchy between the principal façade and secondary public-facing façade.



**4.2.3(e)** Private parking lot owners should consider retrofitting street-fronting parking lots with animated uses (Fig. 87: Tristan Lorei, 2020)



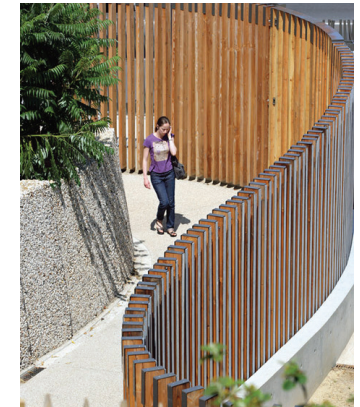
**4.2.3(f-g)** Locate public-facing uses at ground level, and design them with active storefronts. Storefront elements include retail entrances, permeable glazing, and signage positioned at heights designed for pedestrians to read. (Fig. 88: ERA 2020)

**4.2.4 Public works shall conserve the visual and physical presence of the rail corridor in the Railway Transition Zone, and along Railway Street.**

**OBJECTIVE FOR PUBLIC SPACES**

- a. Any development or alteration of public spaces in the Railway Transition Zone should conserve expansive views of the rail corridor.
- b. Safety barriers to the rail corridor are encouraged to be upgraded with creative aesthetic treatments, e.g. framing or highlighting views to the rail corridor, or using materials that interpret the district’s rail heritage (wood, weathering steel).
- c. Wherever possible within existing safety requirements and other constraints, publicly- owned parking lots and open spaces in the Railway Transition Zone should be retrofitted and landscaped as public gathering places.

They should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.



**4.2.4(b)** Upgrade barriers with creative aesthetic treatments to frame, highlight and celebrate rail corridor views. (Figs. 89-90, from left to right: ERA 2020 / Agence Territoires)



**4.2.4(c)** Retrofitting parking lots and open spaces with street furniture and electrical connections allows them to be used for temporary activations like food markets, winter light installations, and more. (Fig. 91: Town of Cochrane)

**4.2.5 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s rail heritage in the Railway Transition Zone, and along Railway Street.**

**OBJECTIVE FOR PUBLIC SPACES**

- a. The Town of Cochrane should explore a Heritage Interpretation Fund (Section 1.4) to deliver and facilitate interpretation in the Railway Transition Zone, and elsewhere in Cochrane.
- b. The Town of Cochrane should engage with diverse stakeholders to develop a Heritage Thematic Framework, which will synthesize the themes of valued cultural heritage in Cochrane.

Future Town-led interpretation should rely on both the Thematic Framework and site-specific studies to determine the most appropriate histories for interpretation on a given site.

- c. The Town of Cochrane should employ diverse media for interpretation within the public realm. These could include:

- Artifact displays
- Audio/video clips
- Images/photographs
- Interpretive design of public-realm elements
- Plaques/panels
- Public art
- Public programming: tours, events, festivals
- Smartphone apps
- Websites

**What is Interpretation?**

Heritage Interpretation refers to the range of strategies (installations, media, activities) that may be used to enhance the public’s awareness and understanding of an historic place. Interpretation allows us to communicate histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

**What’s a Thematic Framework?**

A Heritage Thematic Framework is an historical analysis that synthesizes the valued themes of a place’s history. It is developed through historical research and analysis, and meaningful public engagement. It can be used as a tool to inform conservation decisions and the development of interpretive media.



**4.1.5(c)** Employ diverse media, like large-scale panel installations, to interpret Cochrane’s histories within the public realm (Fig. 92: Cloud Gehshan)

**4.2.5 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s rail heritage in the Railway Transition Zone, and along Railway Street.**

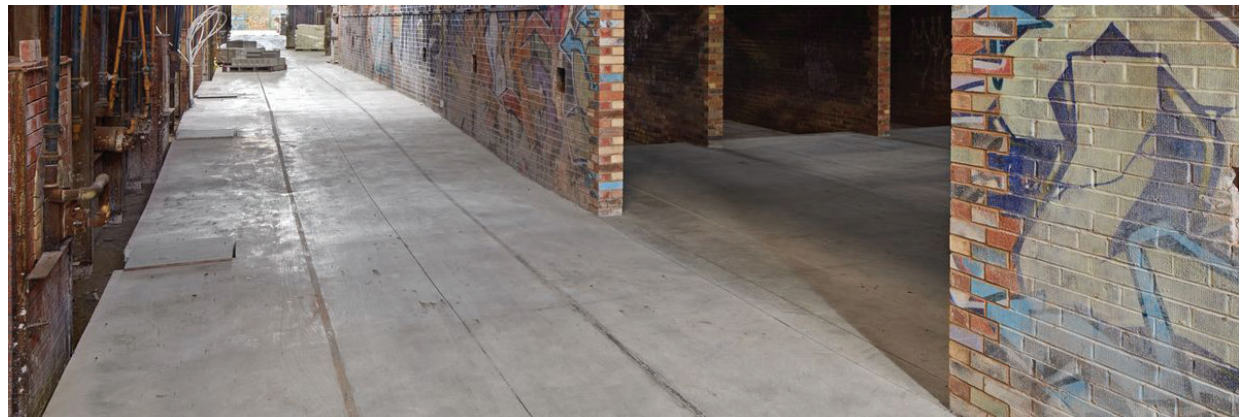
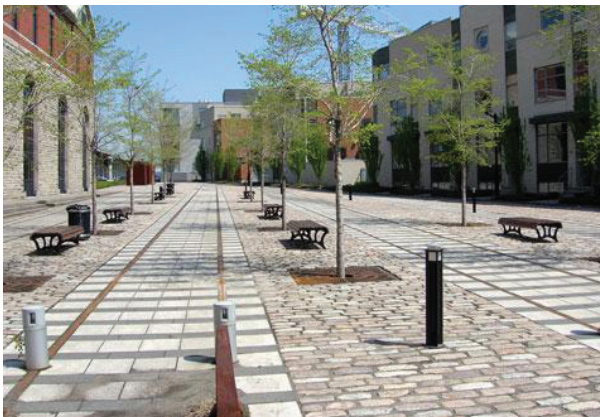
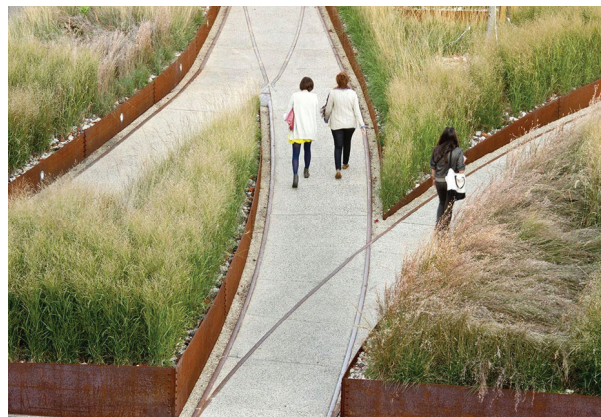
**OBJECTIVE FOR PUBLIC SPACES**



**4.2.5(c)** A diversity of interpretive **public art installations**: standing sculptural pieces, murals, poetry embedded in the sidewalk. (Figs. 93-97, clockwise from top left: Spencer Purdy / J. F. Manzo, 2009 / Town of Cochrane, 2016 / ERA, 2017 / CODAWORX, 2015 )

**4.2.5 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s rail heritage in the Railway Transition Zone, and along Railway Street.**

**OBJECTIVE FOR PUBLIC SPACES**



**4.2.5(c) Interpretive building and landscape design:** public buildings and spaces can be designed with the intent of interpreting key themes in Cochrane’s history. (Figs. 98-102, clockwise from top left: Iwan Baan, 2014 / J. J. Tiziou, Landezine / S9 Architecture / Ben Rahn, 2018 / Pascale Llobat, 2008 )



**OBJECTIVE FOR PUBLIC SPACES**

**4.2.5 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane’s rail heritage in the Railway Transition Zone, and along Railway Street.**

- d. The Town of Cochrane should recognize heritage interpretation as an opportunity to convey the histories that are no longer apparent in the town’s built form, including the traditional practices of First Nations, and railway histories (e.g. location of the former rail station).
- e. The Town of Cochrane should recognize events and programs as methods for heritage interpretation. These could range from historical walking tours to contemporary events and festivals that serve to animate and draw visitors to key hubs in Cochrane.
- f. The Town of Cochrane should capitalize on public spaces and pedestrian trails as key locations for interpretive installations. Consider not only standing installations (panels, sculptures, murals) but public art works inlayed into the ground and/or an interpretive design for the public spaces or trails themselves (e.g. route, landscape materials).
- g. The Town of Cochrane is encouraged to partner with diverse stakeholders to implement and deliver interpretation in the public realm. Relevant stakeholder groups may include First Nations groups, museums, libraries and archives, local business owners, local artists, community organizations, and property owners and developers.

- h. The Town of Cochrane is strongly encouraged to partner with local First Nations to develop an in-depth understanding of the valued cultural heritage associated with the town site and the broader region.



**4.2.5(f)** Capitalize on public spaces and pedestrian trails as key locations for interpretive installations, like engaging panels (left) and ground inlays (right). (Figs. 103-104, from left to right: Geoff Schwartz, 2020 / ERA, 2018)

### **4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along Railway St.**

**OBJECTIVE FOR  
PUBLIC SPACES**

a. The Town of Cochrane should implement key elements to support walkability on new and existing streets, including:

- Sidewalks on both sides of the street
- Small-block street grids
- Regularly-spaced, well-marked pedestrian crosswalks at all intersections, and mid-block crosswalks on larger blocks
- Pedestrian-scale street lighting
- Vehicle-traffic calming measures like curb bumpouts, street trees and crosswalks enhanced with paint or pavers
- Landscaped buffers between sidewalks and the roadway
- Street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding)

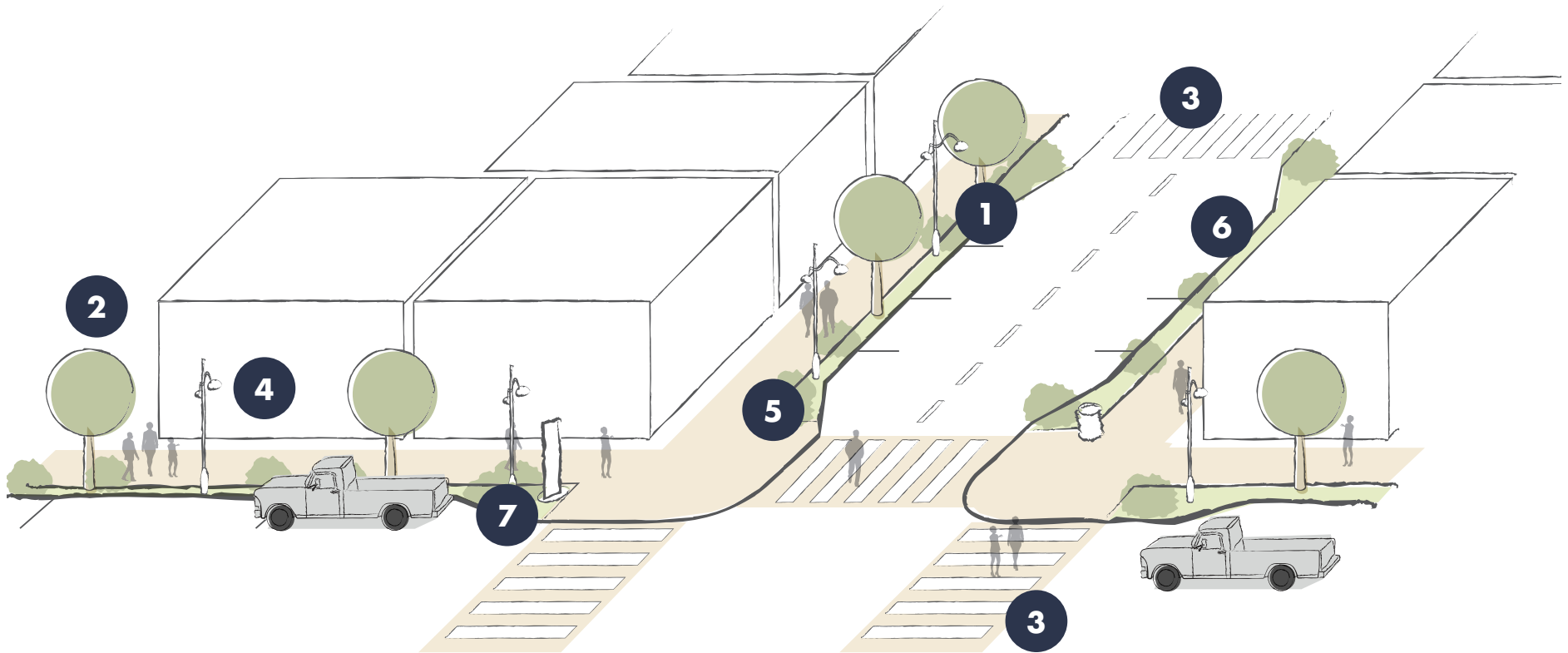
b. The Town of Cochrane is encouraged to consider enhanced public-realm strategies for streets connecting north to First Street, designed to lead toward First Street as a central gathering place for locals and visitors. These might include a distinct roadway paving treatment (e.g. brick pavers) or a curbless woonerf approach where appropriate.



**4.2.6(b)** Consider enhanced public realm strategies for streets connecting up to First Street, with the intent of leading visitors toward First Street as a central gathering place (*Fig. 105: Nice Right Now*)

**4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along Railway St.**

**OBJECTIVE FOR PUBLIC SPACES**



**4.2.6(a)** New and existing streets should support walkability through: (1) sidewalks on both sides; (2) small-block street grids; (3) regular and well-marked pedestrian crosswalks; (4) pedestrian-scale street lighting; (5) traffic calming measures like curb bumpouts, street trees, and painted crosswalks; (6) landscaped buffers between sidewalks and the roadway; and (7) street furniture and amenities. (Fig. 106: ERA 2020)

**4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along Railway St.**

**OBJECTIVE FOR PUBLIC SPACES**

- c. The Town of Cochrane is encouraged to continue to develop and implement a cohesive design language for street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding).
- d. The Town of Cochrane should build on its existing street art program with a town-wide signage/wayfinding strategy that recognizes and celebrates Cochrane as a gathering place for locals and visitors, with:
  - Gateway markers;
  - Interpretive design that references Cochrane’s heritage through imagery and materiality.
- e. Public spaces should be equipped with street furniture and amenities (e.g. seating, lighting, waste receptacles, signage/wayfinding), electrical connections and all-season design to facilitate public use.



**4.2.6(d)** Build on the success of the existing Street Art panels and banners, and consider how they can be scaled to the Railway Transition Zone while prioritizing the Old Town as Cochrane’s key gathering place. (Figs. 107-108: Town of Cochrane / ERA 2020)



**4.2.6(e)** Promote the active use of public spaces with street furniture and amenities, electrical connections, and all-season design. (Fig. 109: ERA, 2020)

**4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along Railway St.**

**OBJECTIVE FOR PUBLIC SPACES**



**4.2.6(e)** Promote the active use of public spaces with street furniture, amenities and other installations designed to engage passersby. (Fig. 110-111, from top to bottom: Landscape Forms / Breathe Architecture)

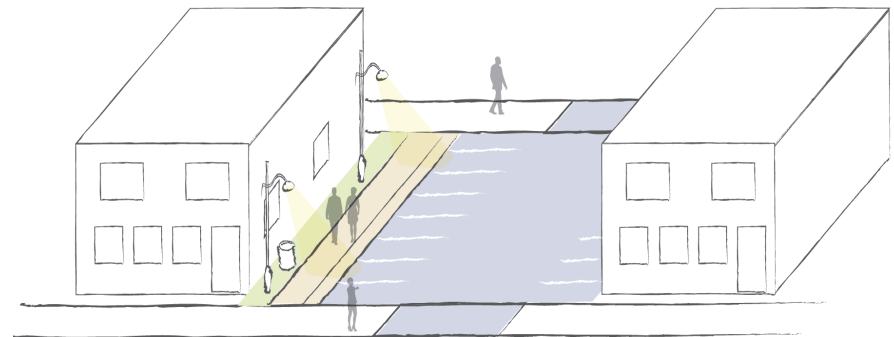


**4.2.6(e)** Consider how to make public spaces engaging and comfortable in all seasons, e.g. through light installations, which may require electrical connections. (Fig. 112: ERA, 2020)

**4.2.6 The public realm and public spaces shall be designed to offer a comfortable pedestrian experience and to facilitate pedestrian-oriented activities in the Railway Transition Zone, and along Railway St.**

**OBJECTIVE FOR PUBLIC SPACES**

- f. Publicly-owned street-fronting parking lots are encouraged to be retrofitted with clearly-marked pedestrian paths and connections to public sidewalks. These paths should be equipped with pedestrian-scale lighting to facilitate their use at all hours.
- g. The Town of Cochrane is encouraged to incentivize and partner with private property owners, businesses and organizations to animate surface parking lots with installations or community events (e.g. pop-up markets, winter skating surfaces).



**4.2.6(f)** Retrofit parking lots with clearly-marked, well-lit pedestrian paths, with safe connections to public sidewalks (Fig. 113: ERA, 2020)



**4.2.6(g)** Develop regular activation of the open spaces along the rail corridor, particularly north of the railway, to bring public activity to the Old Town. (Fig. 114: ERA, 2020)



**4.2.6(g)** Develop regular activation of the open spaces along the rail corridor, particularly north of the railway, to bring public activity to the Old Town. (Fig. 115: Town of Cochrane)



Fig. 116: Tim Hall



## **DESIGN OBJECTIVES: COCHRANE'S CORRIDORS**

*Development projects on private properties are expected to meet the following Objectives throughout Cochrane's Corridors. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

*The Town of Cochrane is expected to meet the following Objectives for projects undertaken in public spaces throughout Cochrane's Corridors. The pages that follow provide Guidelines and visual aids for each Objective to help users interpret and achieve them.*

### **For Development Projects on Private Properties**

### **For Projects in Public Spaces**

4.3.1 New construction shall contribute to the interpretation of Cochrane's heritage, through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.

4.3.2 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane's heritage at key sites along Cochrane's Corridors, and elsewhere in Cochrane.



### **4.3.1 New construction shall contribute to the interpretation of Cochrane's heritage through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.**

- a. A development's interpretive contribution will be provided through an on-site interpretive installation valued at 2.0% of the project's construction costs.
- b. Property owners are expected to familiarize themselves with their site's and area's history and relevant historic themes, using resources including archaeological studies, site-specific historical records, and others made available by the Town of Cochrane, CHAPS and/or the Stockmen Memorial Library.  
This is encouraged early in the development process so that interpretation may be holistically integrated into the site or building design where appropriate.
- c. Property owners are encouraged to consider creative interpretation strategies in lieu of typical plaques and panels. These might include:
  - Art (e.g. sculptures, murals)
  - Images/photographs
  - Interpretive design of buildings or landscape features to evoke themes of the site's history

#### **What is Interpretation?**

Heritage Interpretation refers to the range of strategies (installations, media, activities) that may be used to enhance the public's awareness and understanding of an historic place. Interpretation allows us to communicate histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

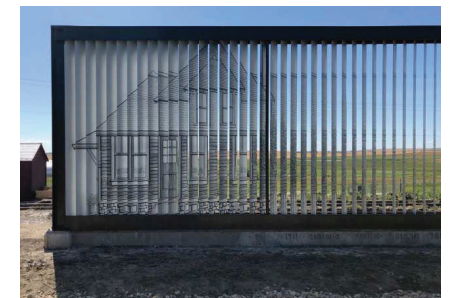
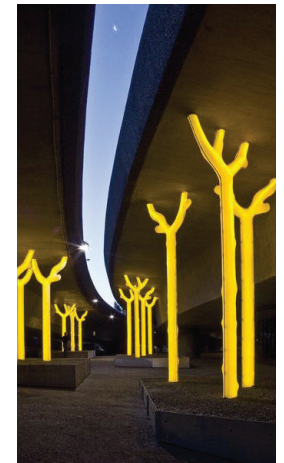


**4.3.1(b)** Engage with CHAPS and other resources (e.g. Glenbow Western Research Centre at the University of Calgary) to learn about Cochrane's history along its intercommunity highways. (Fig. 117: Town of Cochrane, 2016)

**4.3.1 New construction shall contribute to the interpretation of Cochrane's heritage through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.**

- d. On-site interpretive installations along Cochrane's Corridors should be located on the property edge adjacent to Highway 1A, Highway 22 or Griffin Road in order to be visually accessible to both local audiences and commuters / tourists.
- e. On-site interpretive installations along Cochrane's Corridors should be designed to be appreciated by a driving audience. Consider the speed at which viewers will be passing.
- f. On-site interpretation strategies might reference the imagery, materials, forms, motifs and other physical characteristics associated with a site's significant histories and historical themes. Architects, artists and landscape designers are encouraged to consider how these might be subtly integrated into the contemporary development on site.
- g. Property owners are encouraged to engage or partner with local artists (including sculptors, painters, poets, writers, or filmmakers), community organizations, historians and other professionals to develop the interpretive media.

New installations are strongly recommended to embrace understated and/or sophisticated reference to the stories



**4.3.1(d)** Design interpretive installations along Highways 1A and 22 and Griffin Road to be appreciated by a driving audience. Installations should be eye-catching to audiences moving at high speeds. (Figs. 118-121, clockwise from top left: Travel Nevada / City Art Sydney / Spencer Purdy / Travel Alberta / Brian Smith)

### **4.3.1 New construction shall contribute to the interpretation of Cochrane's heritage through on-site strategies such as building design, landscape design, and interpretive media that do not compete with nearby installations.**

they tell; this is best achieved by engaging a professional artist or designer in their development.

- h. On-site interpretive installations should avoid visual noise and competition with adjacent or nearby installations. Consider nearby interpretive installations early in the design process, and develop a strategy that either complements them, or otherwise does not detract from them and/or overwhelm the streetscape.



**4.3.1(f)** On-site interpretation strategies may include building or landscape design to evoke themes of the site's history, e.g. an interpretation of early land forms on site. (Fig. 122: Kearns Mancini Architects, 2015)

**OBJECTIVE FOR PUBLIC SPACES**

**4.3.2 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane's heritage at key sites along Cochrane's Corridors, and elsewhere in Cochrane.**

a. The Town of Cochrane should explore a Heritage Interpretation Fund (Section 1.4) to deliver and facilitate interpretation along Cochrane's Corridors, and elsewhere in Cochrane.

b. The Town of Cochrane should engage with diverse stakeholders to develop a Heritage Thematic Framework, which will synthesize the themes of valued cultural heritage in Cochrane.

Future Town-led interpretation should rely on both the Thematic Framework and site-specific studies to determine the most appropriate histories for interpretation on a given site.

c. The Town of Cochrane should employ diverse media for interpretation within the public realm. These could include:

- Artifact displays
- Audio/video clips
- Images/photographs
- Interpretive design of public-realm elements
- Plaques/panels
- Public art
- Public programming: tours, events, festivals
- Smartphone apps
- Websites

**What is Interpretation?**

Heritage Interpretation refers to the range of strategies (installations, media, activities) that may be used to enhance the public's awareness and understanding of an historic place. Interpretation allows us to communicate histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

**What's a Thematic Framework?**

A Heritage Thematic Framework is an historical analysis that synthesizes the valued themes of a place's history. It is developed through historical research and analysis, and meaningful public engagement. It can be used as a tool to inform conservation decisions and the development of interpretive media.

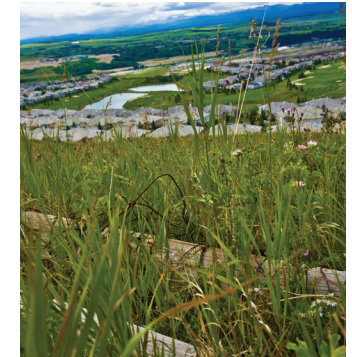


**4.3.2(c)** Employ diverse media, like large-scale mural installations, to interpret Cochrane's histories within the public realm (Fig. 123: Scott Nagy)

**OBJECTIVE FOR PUBLIC SPACES**

**4.3.2 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane's heritage at key sites along Cochrane's Corridors, and elsewhere in Cochrane.**

- d. The Town of Cochrane should recognize heritage interpretation as an opportunity to convey the histories that are no longer apparent in the town's built form, including the traditional practices of First Nations, historic economic drivers (e.g. brick yards, quarries, ranch sites), and inter-community transportation routes
- e. The Town of Cochrane should capitalize on interpretation opportunities adjacent to Highways 1A and 22, and Griffin Road, to convey Cochrane's western heritage to an audience of travelers and commuters. It could include elements like murals or sculptures designed to be legible from moving vehicles, or rural-style landscape design (e.g. ranch fencing, green fields).
- f. The Town of Cochrane should capitalize on public spaces and pedestrian trails as key locations for interpretive installations. Consider not only standing installations (panels, sculptures, murals) but public art works inlayed into the ground and/or an interpretive design for the public spaces or trails themselves (e.g. route, landscape materials).
- g. The Town of Cochrane should explore the interpretation of histories associated with significant natural features in town like the Bow River and the Big Hill.



**4.3.2(e)** Simple interpretive installations like ranch fencing and native grass landscaping will go a long way in cultivating a picturesque rural environment along Highways 1A and 22. Consider how well-designed landscaping may be used creatively to highlight or screen building forms. (Figs. 124-125: Town of Cochrane)



**4.3.2(f)** Capitalize on public spaces and pedestrian trails as key locations for interpretive installations, like engaging panels (left) and ground inlays (right). (Figs. 126-127, from left to right: Geoff Schwartz, 2020 / ERA, 2018)

**4.3.2 The Town of Cochrane shall use diverse media to deliver and facilitate the interpretation of Cochrane's heritage at key sites along Cochrane's Corridors, and elsewhere in Cochrane.**

**OBJECTIVE FOR PUBLIC SPACES**

h. The Town of Cochrane is encouraged to build on its street art program with a town-wide signage/wayfinding strategy. It should be expressed along Cochrane's Corridors in a design language consistent with the Old Town and Railway Transition Zone.

Signage and wayfinding should be implemented at:

- Gateways into Cochrane;
- Key highway locations to direct visitors to Cochrane's historic town site. The strategy will capitalize on the value of the Old Town and Railway Transition Zone in conveying Cochrane's early settlement history and rail heritage.

i. The Town of Cochrane is encouraged to partner with diverse stakeholders to implement and deliver interpretation in the public realm. Relevant stakeholder groups may include First Nations groups, museums, libraries and archives, local business owners, local artists, community organizations, and property owners and developers.

j. The Town of Cochrane is strongly encouraged to partner with local First Nations to develop an in-depth understanding of the valued cultural heritage associated with the town site and the broader region.



**4.2.7(d)** Build on the success of the existing Street Art panels and banners, and consider how they can be scaled to Cochrane's Corridors in ways that send visitors to the Old Town as Cochrane's key gathering place. (Figs. 128-129: Town of Cochrane / ERA 2020)



COCHRANE ALTA

Fig. 130: CHAPS CH 010-0203

# 5.0 IMPLEMENTATION

## 5.1 The Development Review Process

In Cochrane, proposed developments are subject to a review process, during which the Town of Cochrane will determine whether a proposed development successfully meets the Western Heritage (WH) Design Objectives to which it may be subject.

The proponent of a development project is recommended to begin the process by initiating a Pre-Application Meeting, where a Town of Cochrane Development Officer will identify all regulations and policies with which the proposed project is expected to comply, including any applicable WH Objectives.

**Any development application that falls within a WH Character Area must include a Design Brief**, where the proponent or their consultant must demonstrate how the application meets the Design Objectives.

Once an application is submitted, the Development Officer will review the application against all applicable regulations, including the WHDF, and will circulate the application to any other relevant departments for comment.

Applications involving a permitted use that meet all regulations of the Land Use Bylaw, including the WH Objectives, will be approved by the Development Officer.

Applications that do not meet the WH Design Objectives, under the Development Officer’s interpretation, will be refused. The Development Officer will work to help the applicant understand how the proposal may be revised.

Applications involving a discretionary use / variance under the Land Use Bylaw may be sent by the Development Officer to Cochrane Planning Commission (CPC) for review.

A Development Officer may also send an application to CPC if it is unclear whether the proposal meets the WH Objectives.

CPC may issue or refuse a development permit.

Any property owner affected by a decision of a Development Officer or CPC may appeal to the Subdivision & Development Appeal Board.

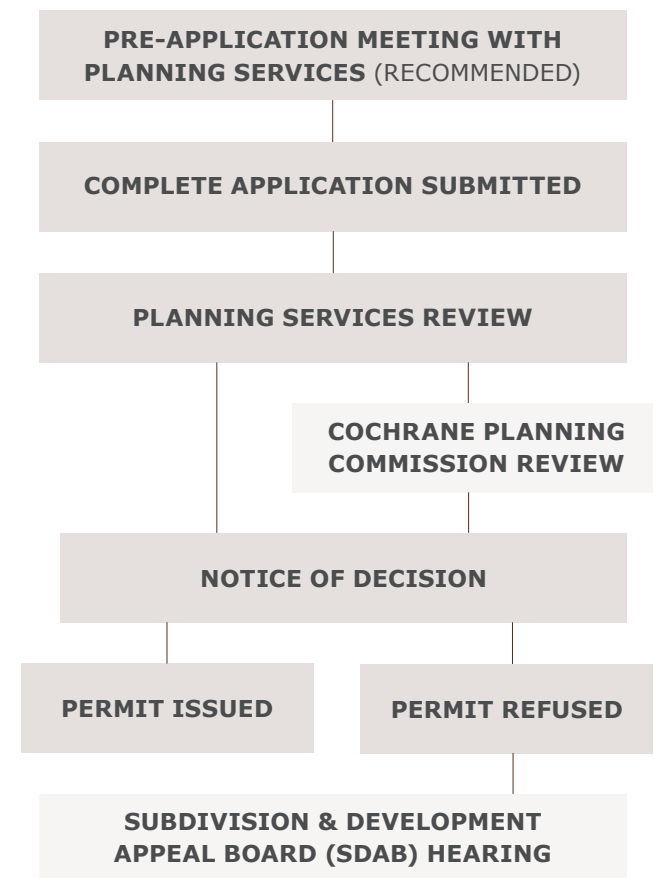


Fig. 131: ERA 2020





Fig. 132: Town of Cochrane

## 6.0 DEFINITIONS

The following defined terms may assist users in understanding the Western Heritage Design Framework (2022).

**Addition:** a physical or structural extension of an existing building.

**Adjacent:** land that is contiguous to a parcel of land, and includes:

- i) land that would be contiguous if not for a highway, road, river or stream, and,
- ii) any other land identified in the land use bylaw as adjacent land.

*(Town of Cochrane Land Use Bylaw)*

**All-Season Design:** physical features or amenities that facilitate use of a space throughout the year, e.g. shade structures, water fountains, splash pads, electrical outlets, landscaping to provide shade or windbreaks.

**Alteration:** any renovation or change to an existing building.

**Animated:** spaces that are designed and/or programmed to appear active or engaging to passersby.

**Bay (building):** any division of a building between vertical lines or planes.

**Character-Defining Elements:** the materials, forms, location, spatial configurations, uses and cultural associations or meanings that contribute to the heritage value of an historic place, which must be retained in order to preserve its heritage value. *(Standards and Guidelines for the Conservation of Historic Places in Canada, 2010)*

**Cochrane Heritage Register:** a inventory of properties containing historic resources that have been found to carry cultural heritage value and have been recognized by the Town of Cochrane as priorities for retention and conservation, but have not been designated as Municipal Historic Resources.

**Cochrane Planning Commission:** a seven-member body (five public appointees, and two members of Council) responsible for exercising development powers and duties on behalf of the Town of Cochrane. *(Cochrane Planning Commission Terms of Reference)*

**Conservation:** all actions or processes that are aimed at safeguarding the character-defining elements of a cultural resource so as to retain its heritage value and extend its physical life. This may involve 'Preservation,' 'Rehabilitation,' 'Restoration,' or a combination of these actions or processes. *(Standards and Guidelines for the Conservation of Historic Places in Canada, 2010)*

**Datum Line:** a continuous horizontal element or plane on a building that serves as a reference point for the design of vertical building elements.

**Design Brief:** for the purposes of the *Western Heritage Design Framework (2022)*, a written memo that accompanies architectural / landscape drawings to identify or describe aspects of a design proposal.

**Design Guideline:** for the purposes of the *Western Heritage Design Framework (2022)*, a non-mandatory suggestion or recommendation intended to assist users in understanding how to achieve the associated mandatory Design Objective.

**Design Objective:** for the purposes of the *Western Heritage Design Framework (2022)*, a mandatory direction that public and private development projects are required to meet. Design Objectives are phrased to be high-level and open-ended to allow for diverse strategies in achieving them. Development approval authorities will employ their judgement and consult the associated Design Guidelines to help determine whether the Design Objective is successfully achieved.

**Development Project:** for the purposes of the *Western Heritage Design Framework (2022)*, Development Projects include new construction, additions on existing buildings, alterations to existing buildings, public works, and the creation or renovation of public spaces.

**Documentary Evidence:** in heritage restoration, evidence of a building or site's previous appearance that has been documented through resources like archival photographs and original architectural drawings and specifications.

**Front Setback:** see Setback (front).

**Heritage Interpretation:** the range of strategies (installations, media, activities) that may be used to enhance the public's awareness and understanding of an historic place. Interpretation allows for the communication of histories that cannot be understood from the existing built environment, like built features that have since been lost, or intangible cultural traditions and practices.

**In kind:** matching, i.e. with the same form, material and detailing as seen in the existing elements. (*Standards and Guidelines for the Conservation of Historic Places in Canada, 2010*)

**Intangible Cultural Heritage:** the practices, representations, expressions, knowledge, skills - as well as the instruments, objects, artefacts and cultural spaces associated therewith - that communities, groups, and in some cases, individuals recognize as part of their cultural heritage. (*International Council on Monuments and Sites*)

**Integrity:** the degree to which the property has been altered since originally constructed and designed. The reversibility of alterations should also be taken into account. (*Cochrane Heritage Register, 2002*)

**Interpretation:** see Heritage Interpretation.

**Interpretive Media:** media used to enhance the public's awareness and understanding of an historic place. Interpretive media can include (but may not be limited to) artifact displays, audio/video clips, images and photographs, interpretive design of public realm elements, plaques and panels, public art installations, public programming like tours, events and festivals, smartphone apps, and websites.

**Mixed Use:** developments incorporating a mix of land uses, typically residential units with commercial units at grade.

**Parapet (roof):** a low wall built along the edge of a roof. Parapets are a typical feature of Western boomtown-era architecture.

**Pedestrian scale:** building- and public-realm elements designed at a scale (size, height) suited to pedestrians, rather than drivers. Examples include the positioning of retail signage at pedestrian eye levels, or the selection of street lights scaled to light sidewalks as well as roads.

**Permeable/permeability:** in storefront design, the use of glazing (windows) to allow clear views into the ground-floor unit(s) from the public realm. Permeable storefronts

are considered a key strategy in animating public streets for comfortable and engaging pedestrian use.

**Physical Evidence:** in heritage restoration, evidence of a building or site's previous appearance that can be determined through on-site investigations, e.g. paint analyses.

**Pitched Roof:** a roof that is sloped, rather than flat. Examples of pitched roof styles include gabled roofs, hipped roofs, and mansard roofs.

**Preservation:** the actions or process of protecting, maintaining, and/or stabilizing the existing materials, form, and integrity of a historic place or of an individual component, while protecting its heritage value. (*Standards and Guidelines for the Conservation of Historic Places in Canada, 2010*)

**Public realm:** for the purposes of the *Western Heritage Design Framework (2022)*, publicly-owned outdoor lands, including sidewalks, roadways and laneways, other public thoroughfares, public parks and squares, and other publicly-owned outdoor areas.

**Public spaces:** for the purposes of the *Western Heritage Design Framework (2022)*, publicly-owned outdoor lands, typically in reference to public parks, parkettes and squares.

**Public works:** public infrastructural projects like roadway and servicing improvements, park revitalizations, and other projects undertaken in the public realm.



**Rehabilitation:** the action or process of making possible a continuing or compatible contemporary use of a historic place or an individual component, while protecting its heritage value. (*Standards and Guidelines for the Conservation of Historic Places in Canada, 2010*)

**Restoration:** the action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value. (*Standards and Guidelines for the Conservation of Historic Places in Canada, 2010*)

**Retention (building):** for the purposes of the *Western Heritage Design Framework (2022)*, building retention ranges from the retention of one facade of the existing building, to full retention of all exterior elevations and interior features.

**Setback (front):** the space between a building’s front wall and front property line.

**Setback (side):** the space between a building’s side wall and side property line.

**Setback (zero-lot-line):** a condition where a building is built along the lot’s property line.

**Stepback:** in architecture, a step-like recession in a building wall, often used to set a building’s upper storeys further back from the property line or streetwall.

**Streetwall:** the vertical elements, typically a series of buildings, that form the edges of a street.

**Side Setback:** see Setback (side).

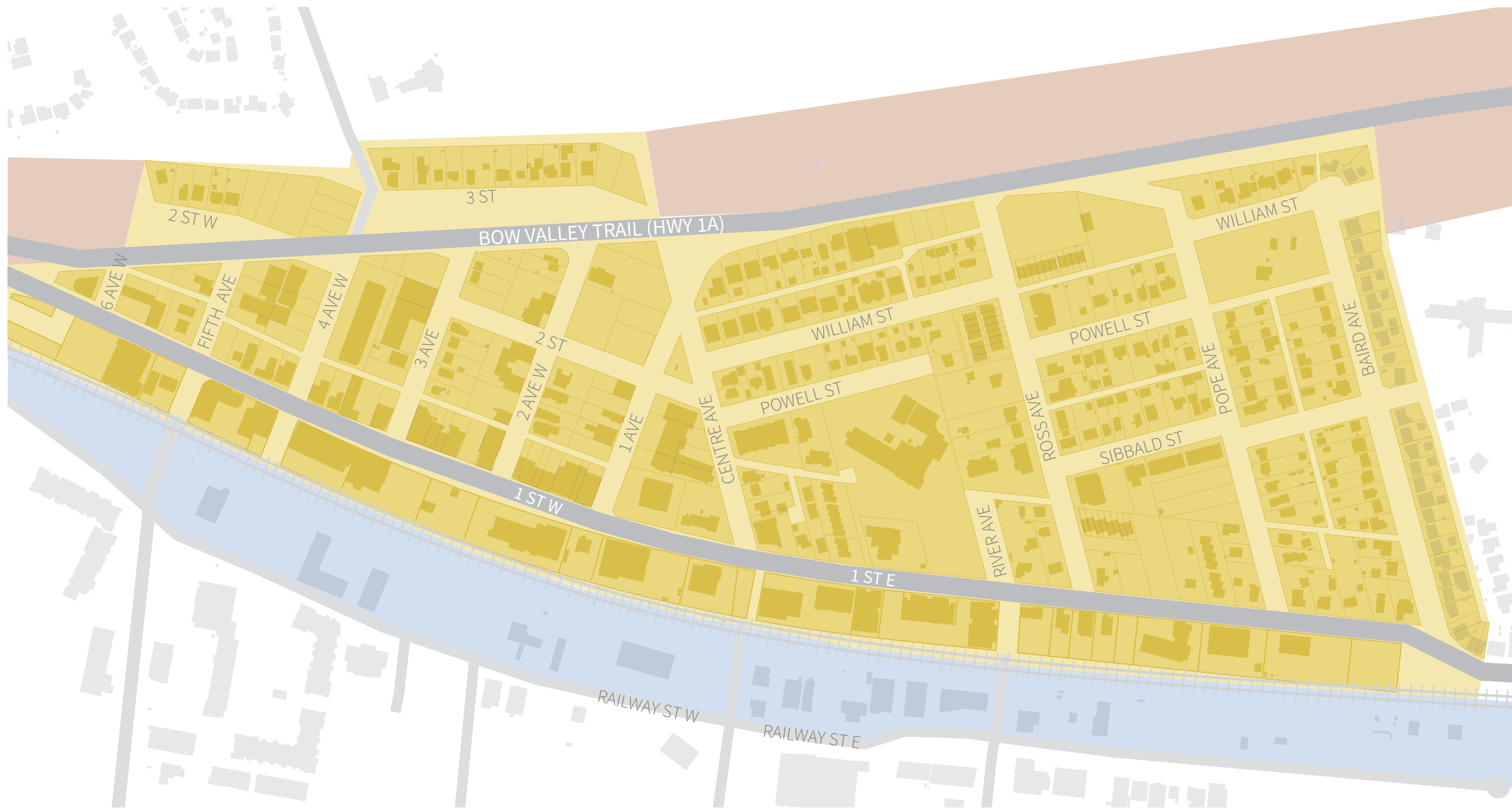
**Subdivision and Development Appeal Board:** an eight-member body (six public appointees, and two members of Council) responsible for hearing and determining appeals on behalf of the Town in respect to decisions of a Subdivision Authority or Development Authority. (*Cochrane Subdivision & Development Appeal Board Terms of Reference*)

**Woonerf:** also known as a “shared street,” a curbsless street designed for use by pedestrians, cyclists and vehicles at low speeds. Often includes various forms of vehicle traffic-calming measures, such as bollards, street furniture, planters, textured paving and lay-by street parking.

**Zero Lot Line Setback:** see Setback (zero-lot-line).



Fig. 133: Tim Hall



**LEGEND**

- OLD TOWN
- PROPERTIES WITHIN ZONE
- RAILWAY TRANSITION ZONE
- BUILDING FOOTPRINT
- RAILWAY
- COCHRANE'S CORRIDORS

**OLD TOWN**  
SCHEDULE A - OLD TOWN

WESTERN HERITAGE DESIGN FRAMEWORK

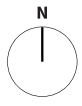




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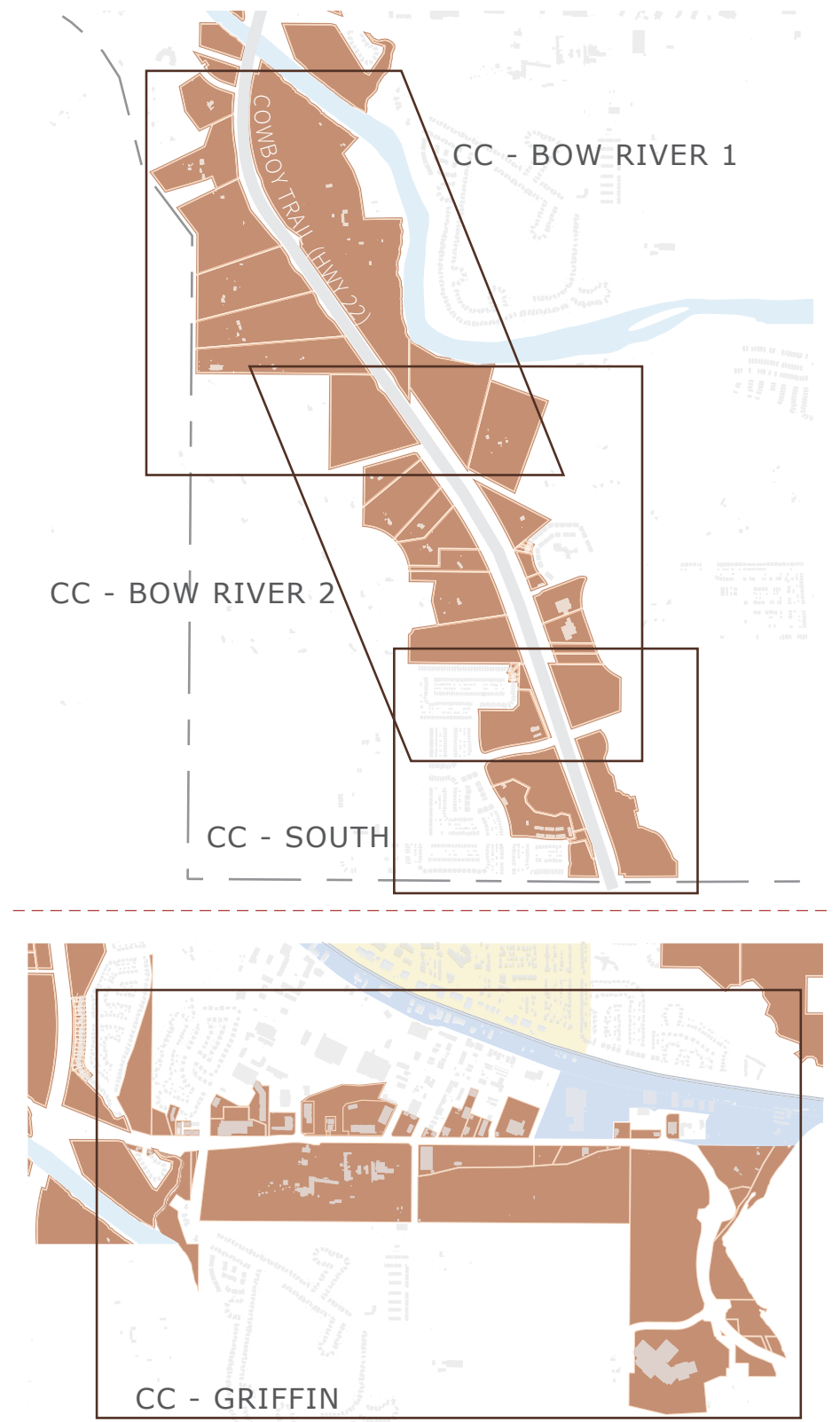
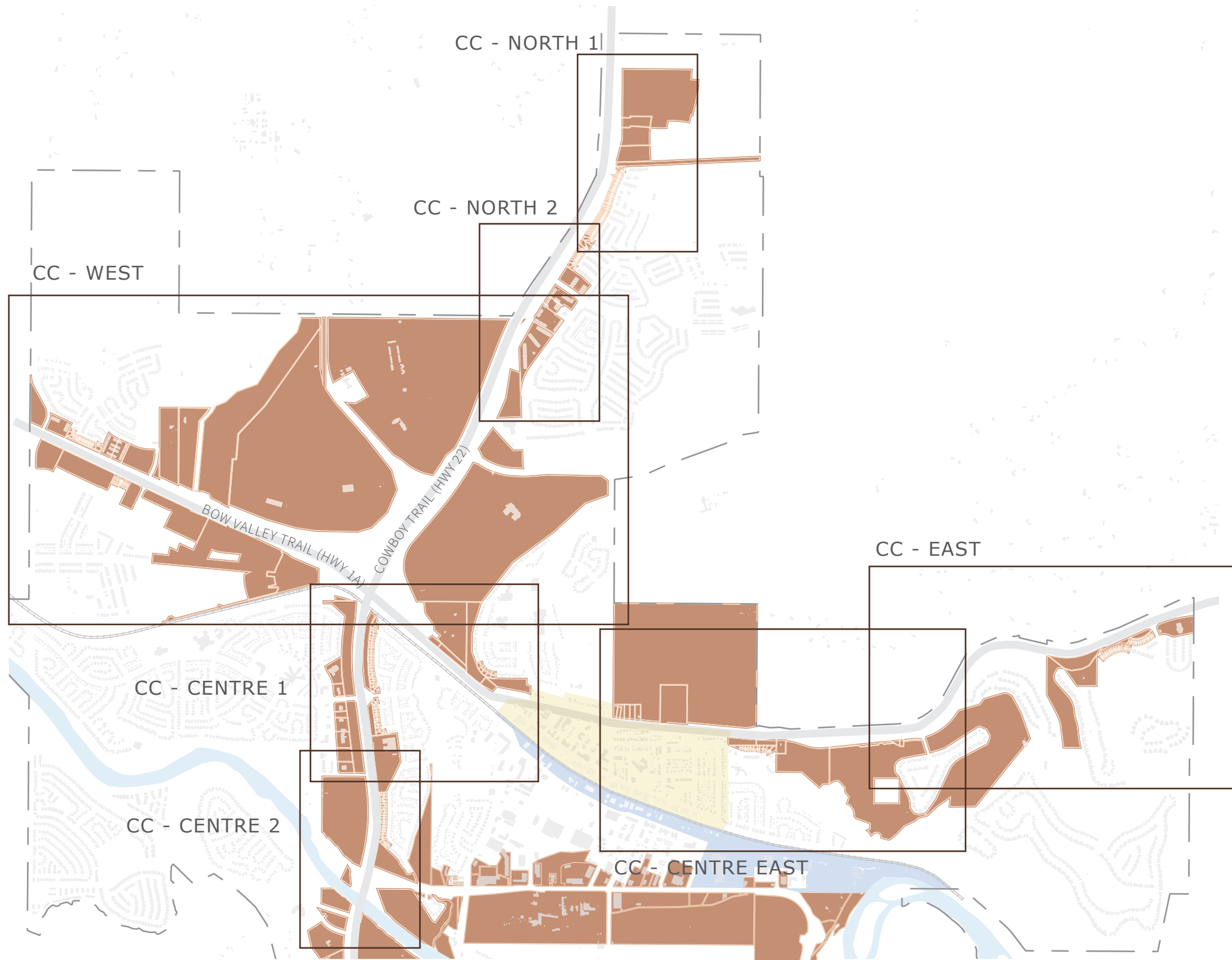
- RAILWAY TRANSITION ZONE
- OLD TOWN
- COCHRANE'S CORRIDORS
- BUILDING FOOTPRINT
- RAILWAY

**RAILWAY TRANSITION ZONE**  
 SCHEDULE B - RAILWAY TRANSITION ZONE



WESTERN HERITAGE DESIGN FRAMEWORK

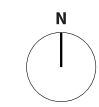




**LEGEND**

- PROPERTIES WITHIN COCHRANE'S CORRIDORS
- OLD TOWN
- RAILWAY TRANSITION ZONE
- BUILDING FOOTPRINT
- RAILWAY
- COCHRANE BOUNDARY
- BOW RIVER

**CC - KEY MAP**  
APPENDIX C - COCHRANE'S CORRIDORS





**LEGEND**



PROPERTIES WITHIN COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY

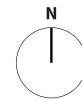


COCHRANE BOUNDARY



BOW RIVER

**CC - WEST**  
APPENDIX C - COCHRANE'S CORRIDORS



**cochrane**  
HOW THE WEST IS NOW

WESTERN HERITAGE DESIGN FRAMEWORK

NORTH 1

NORTH 2

COWBOY TRAIL (HWY 22)

COWBOY TRAIL (HWY 22)

SUNRISE COMMON

SUNRISE CRES

SUNRISE CRES

SUNRISE CRES

SUNRISE TERR

SUNRISE VIEW

SUNSET RD

SUNRISE CRES

SUNSET DR

SUNSET CIRCLE

SUNSET DR

SUNSET CLOSE

SUNSET BLVD

SUNSET DR

SUNSET HEIGHTS

SUNRISE VIEW

SUNSET SQ

LEGEND



PROPERTIES WITHIN COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY

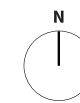


COCHRANE BOUNDARY



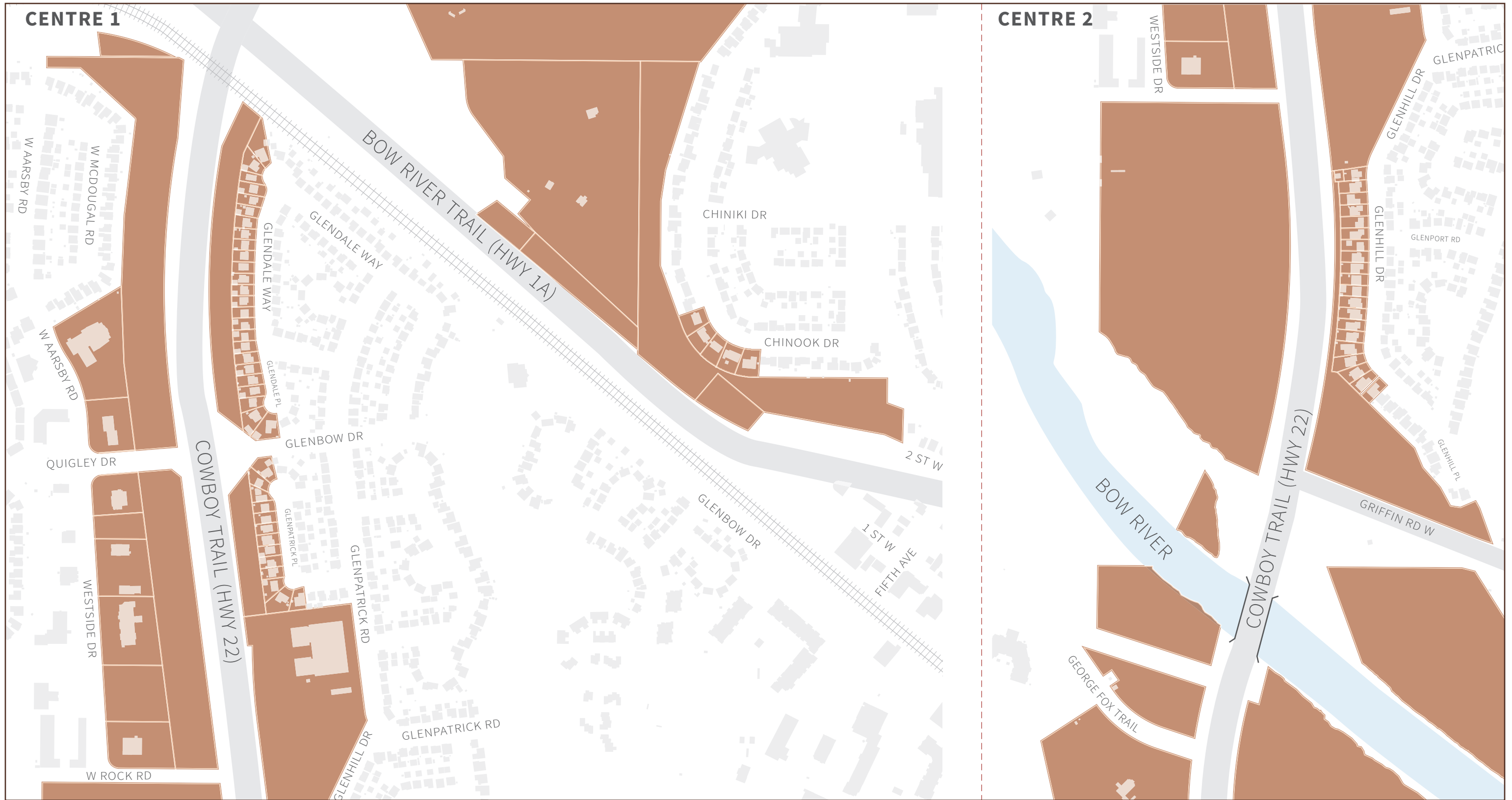
BOW RIVER

CC - NORTH  
APPENDIX C - COCHRANE'S CORRIDORS



cochrane  
HOW THE WEST IS NOW

WESTERN HERITAGE DESIGN FRAMEWORK



**LEGEND**



PROPERTIES WITHIN COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY

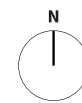


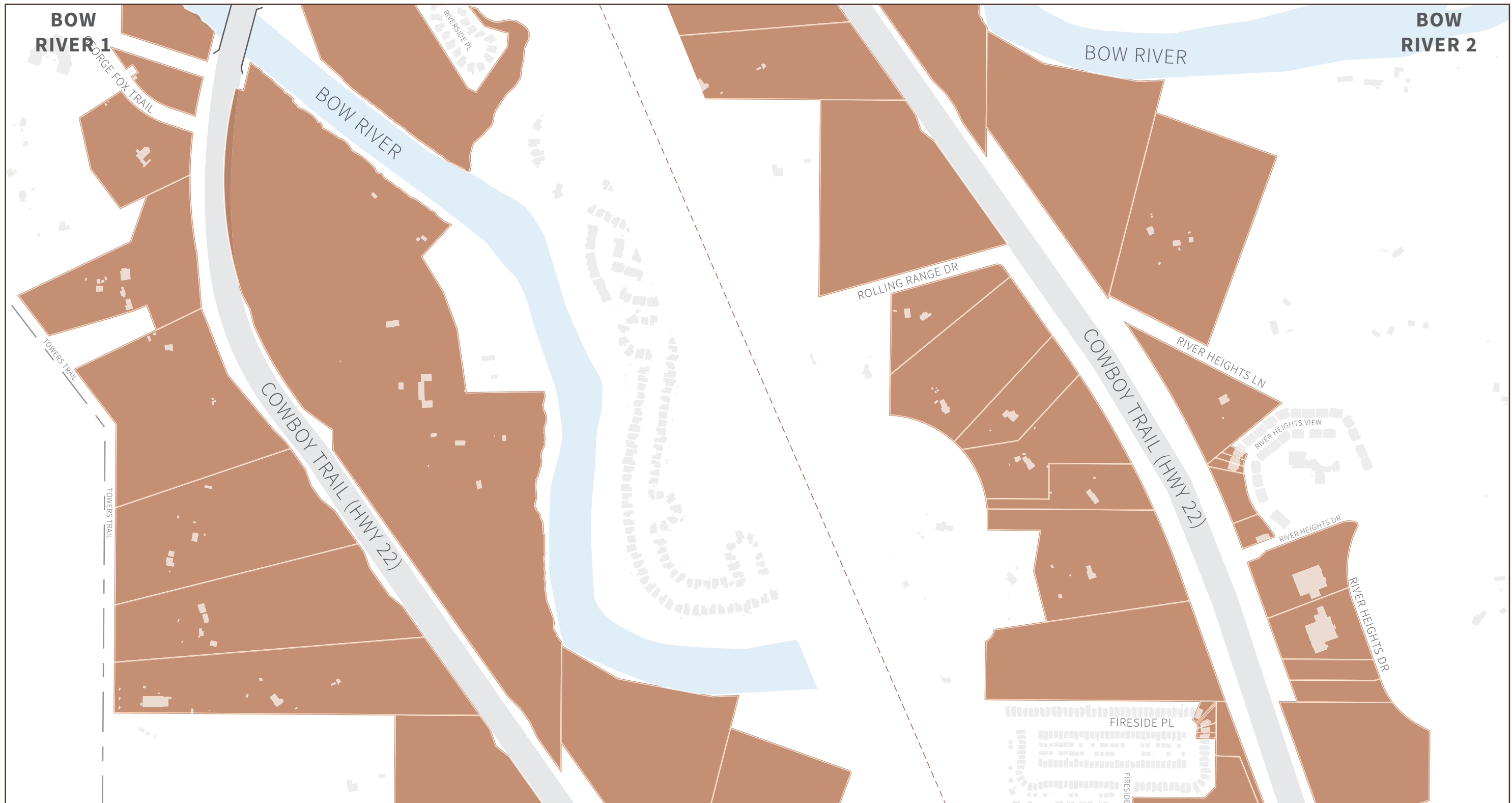
COCHRANE BOUNDARY



BOW RIVER

**CC - CENTRE**  
APPENDIX C - COCHRANE'S CORRIDORS





**LEGEND**

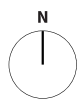
- COCHRANE'S CORRIDORS

PROPERTIES WITHIN ZONE
- BUILDING FOOTPRINT

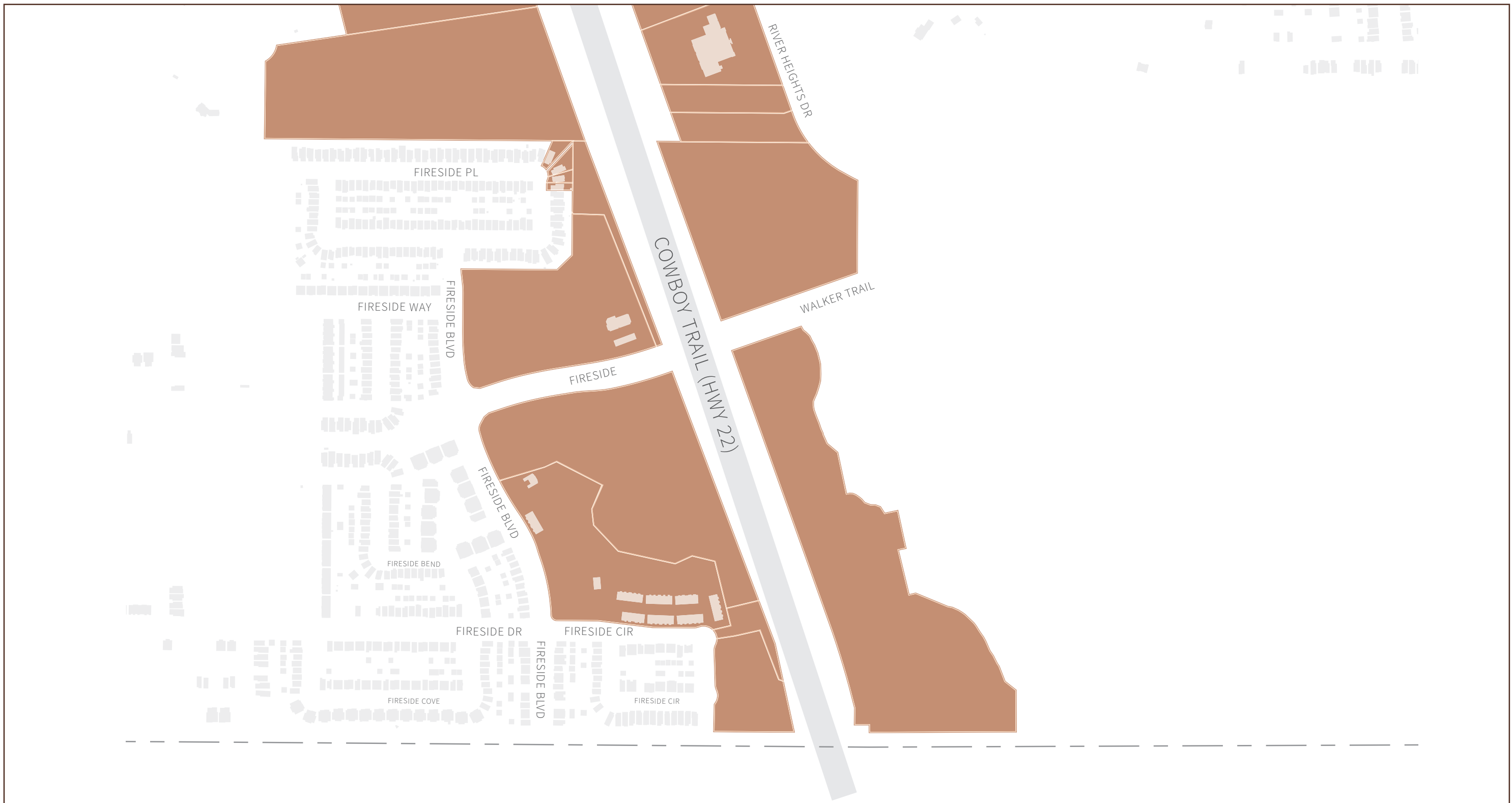
RAILWAY
- COCHRANE BOUNDARY

BOW RIVER

**CC - BOW RIVER**  
**APPENDIX C - COCHRANE'S CORRIDORS**



WESTERN HERITAGE DESIGN FRAMEWORK



**LEGEND**



PROPERTIES WITHIN  
COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY



COCHRANE BOUNDARY

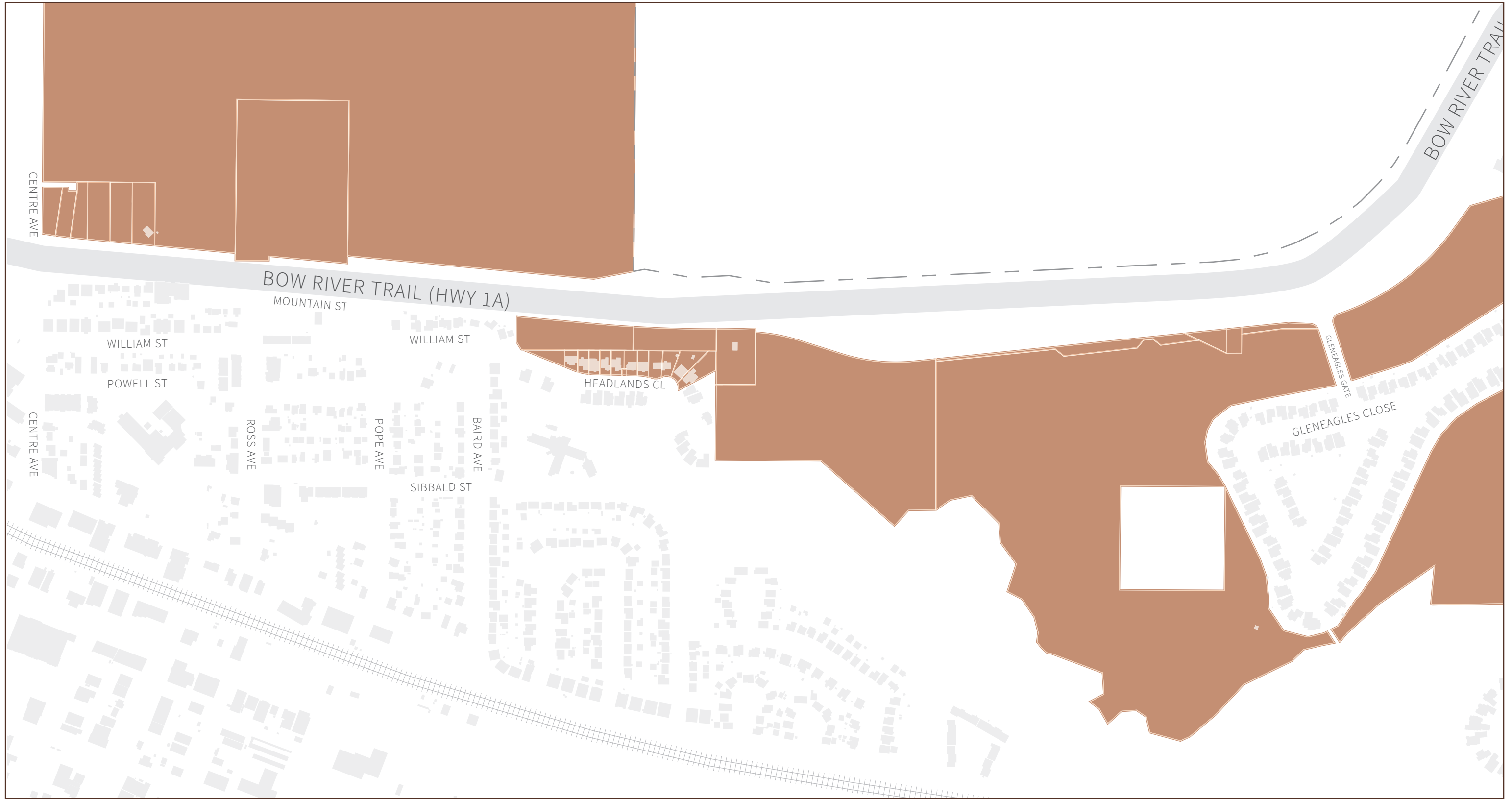


BOW RIVER

**CC - SOUTH**  
APPENDIX C - COCHRANE'S CORRIDORS



WESTERN HERITAGE DESIGN FRAMEWORK



**LEGEND**



PROPERTIES WITHIN COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY



COCHRANE BOUNDARY



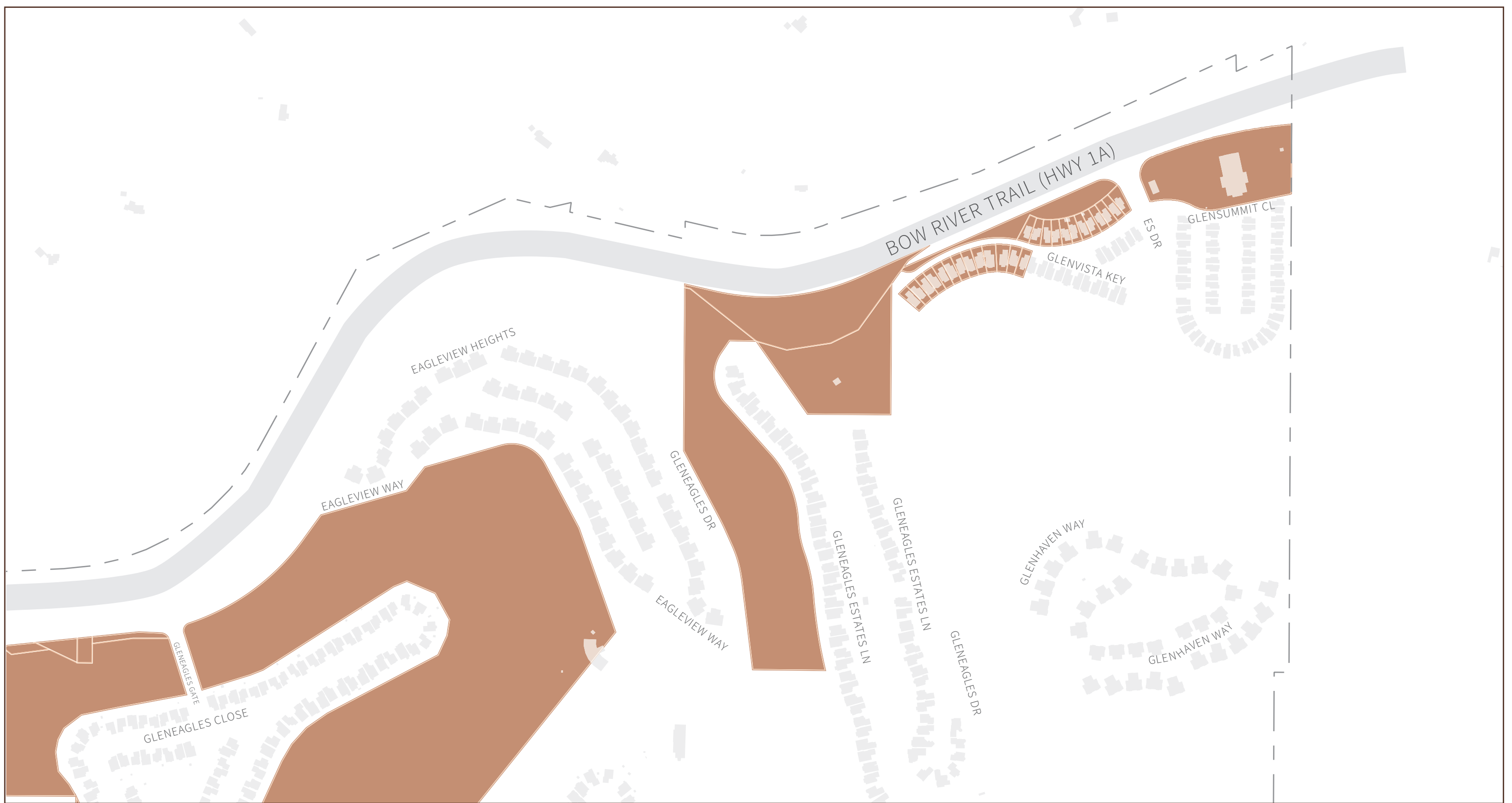
BOW RIVER

**CC - CENTRE EAST**  
APPENDIX C - COCHRANE'S CORRIDORS





WESTERN HERITAGE DESIGN FRAMEWORK







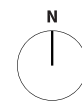
**LEGEND**

 PROPERTIES WITHIN COCHRANE'S CORRIDORS

 BUILDING FOOTPRINT  
 RAILWAY

 COCHRANE BOUNDARY  
 BOW RIVER

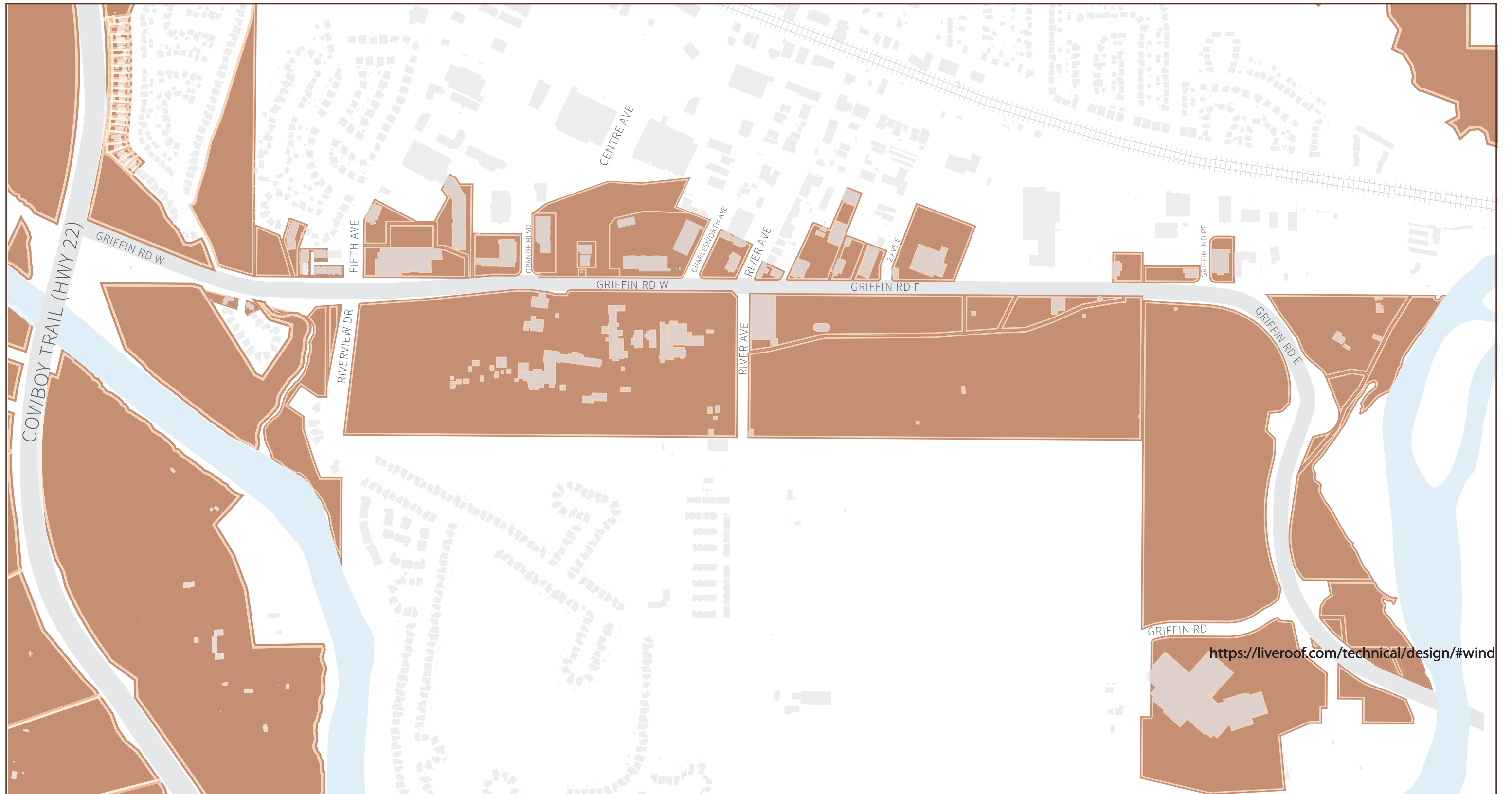
**CC - EAST**  
**APPENDIX C - COCHRANE'S CORRIDORS**



WESTERN HERITAGE DESIGN FRAMEWORK







**LEGEND**



PROPERTIES WITHIN  
COCHRANE'S CORRIDORS



BUILDING FOOTPRINT



RAILWAY



COCHRANE BOUNDARY



BOW RIVER

**CC - GRIFFIN**  
APPENDIX C - COCHRANE'S CORRIDORS



WESTERN HERITAGE DESIGN FRAMEWORK